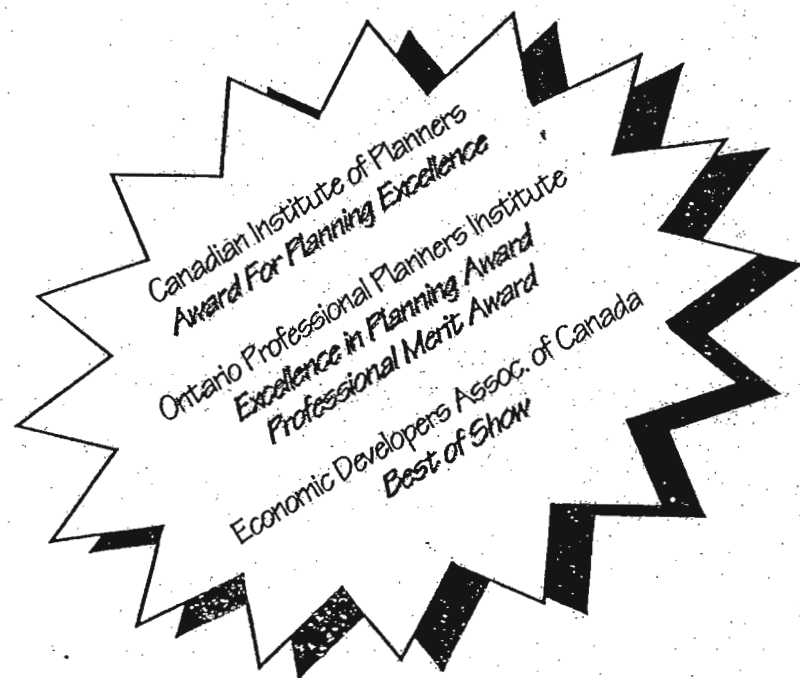
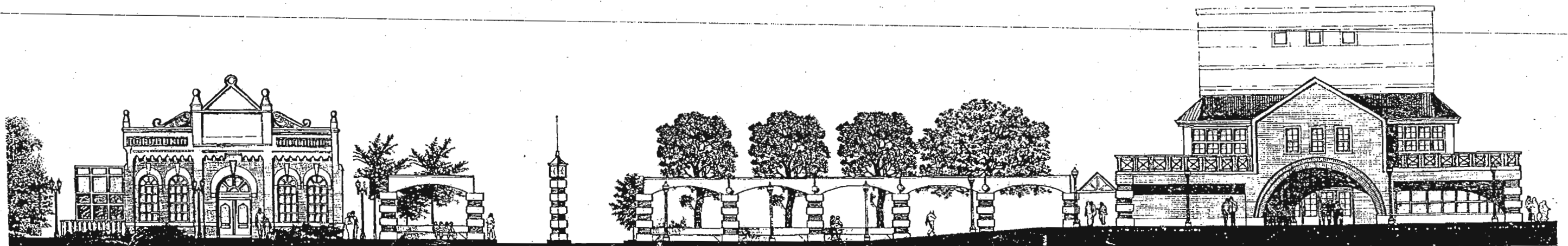


City of Stoney Creek



Prepared by:

City of Stoney Creek Planning Dept.
Rino Mostacci, MCIP, RPP, Senior Planner
Sinisa Tomic, BA, Urban Designer



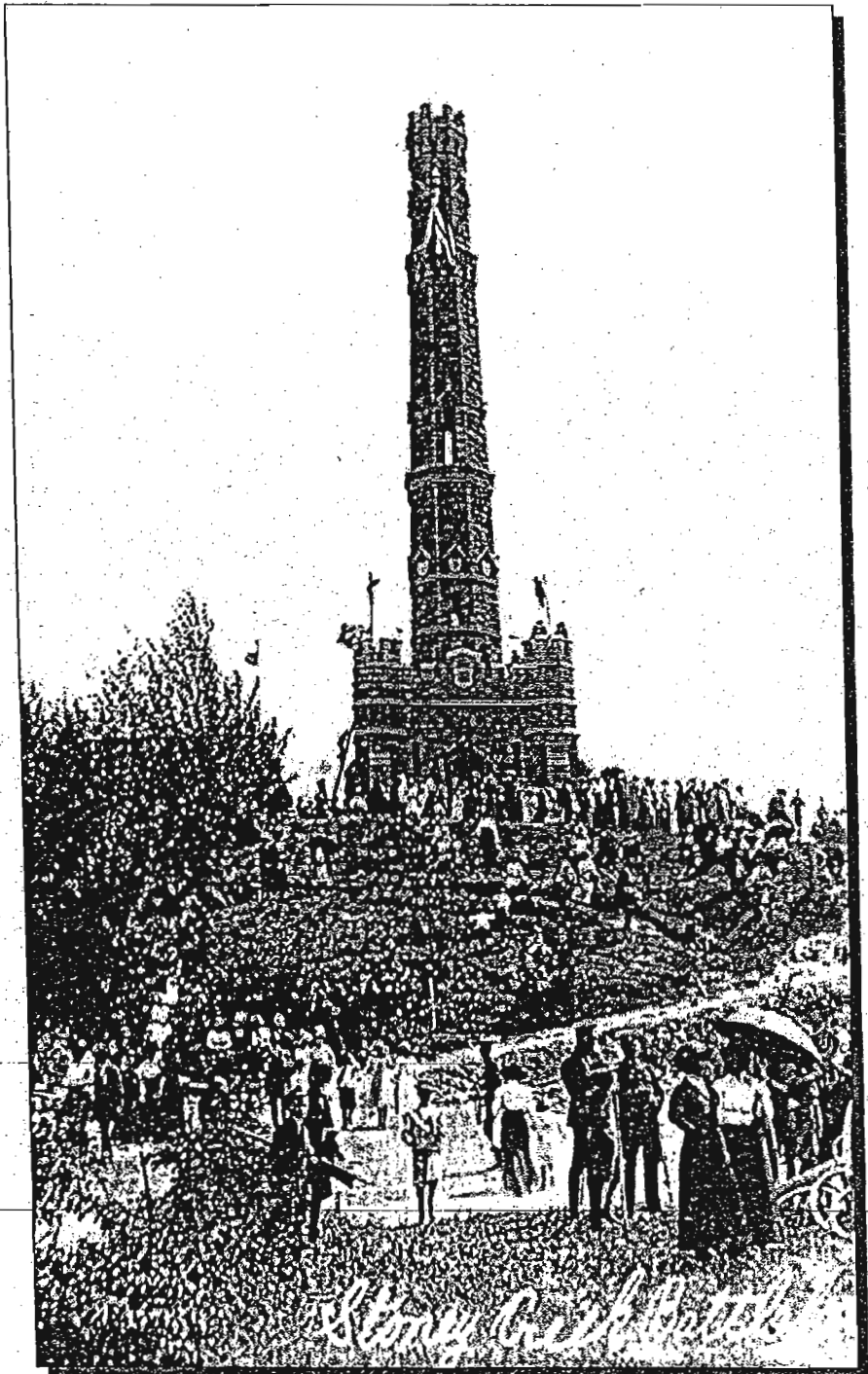
Olde Town Urban Design Plan

TABLE OF CONTENTS

1.0	Goals of The Urban Design Plan	1
2.0	Introduction & Background.....	2
2.1	City of Stoney Creek	3
2.2	Story of Stoney Creek.....	4
3.0	Urban Design Vision.....	6
3.1	Existing Site Plan - Downtown	7
3.2	Future Site Plan - Downtown.....	8
4.0	The Streetscape	9
4.1	Design Theme.....	11
4.2	Sidewalks & Crosswalks.....	14
4.3	On-street Parking	18
4.4	Landscaping & Planting	19
4.5	Facades & Signage.....	21
4.6	Lighting & Utility Wires	22
4.7	Furnishings & Amenities.....	23
5.0	Public Spaces & Landmarks	24
5.1	Town Square	25
5.2	Jones Street Bridge	27
5.3	Jones Street Pedestrian Mall.....	28
5.4	Stoney Creek Bridge.....	29
5.5	Olde Town Centre and Market Area.....	31
5.6	King Street Boulevard/Median	32
5.7	Community Park Complex	33
5.8	Smith's Knoll Memorial.....	35
5.9	Battlefield Park and Bridge.....	37
6.0	Elevations.....	40
6.1	King Street, North & South Elevations	41
6.2	Jones Street, North Elevation.....	47
6.3	Jones Street, South Elevation	51
6.4	Jones Street, West Elevation.....	54
7.0	Implementation Schedule	56
7.1	Near Term, 1998 - 2000.....	57
7.2	Medium Term, 2001 - 2006.....	58
7.3	Long Term, 2007 - 2012.....	59
7.4	Public - Private Projects.....	60

Battle of Stoney Creek Monument, War of 1812

The Monument is unique in Canada and has become a symbol of The City of Stoney Creek. It was dedicated on June 6, 1913 to commemorate the Centennial of The Battle of Stoney Creek and 100 years of peace between Canada and the United States. The monument has a height of 30.5 meters and is constructed of Queenston limestone.



Dedication of the Monument 1913

- To enhance the aesthetic and functional character of the historical Downtown.
- To establish "sense of place" by way of a sequence of high quality public spaces.
- To establish a common vision to guide future actions and investment.
- To make the historical Downtown the identifiable centre of the City of Stoney Creek.
- To create a uniform and harmonious environment through streetscape treatment and to create pedestrian linkages.
- To establish gateways into the Downtown Core, enhance the Stoney Creek and embellish the bridges and landmarks in the area.
- To implement this Plan in time for the Bicentennial of Battle of Stoney Creek (2013).

Introduction

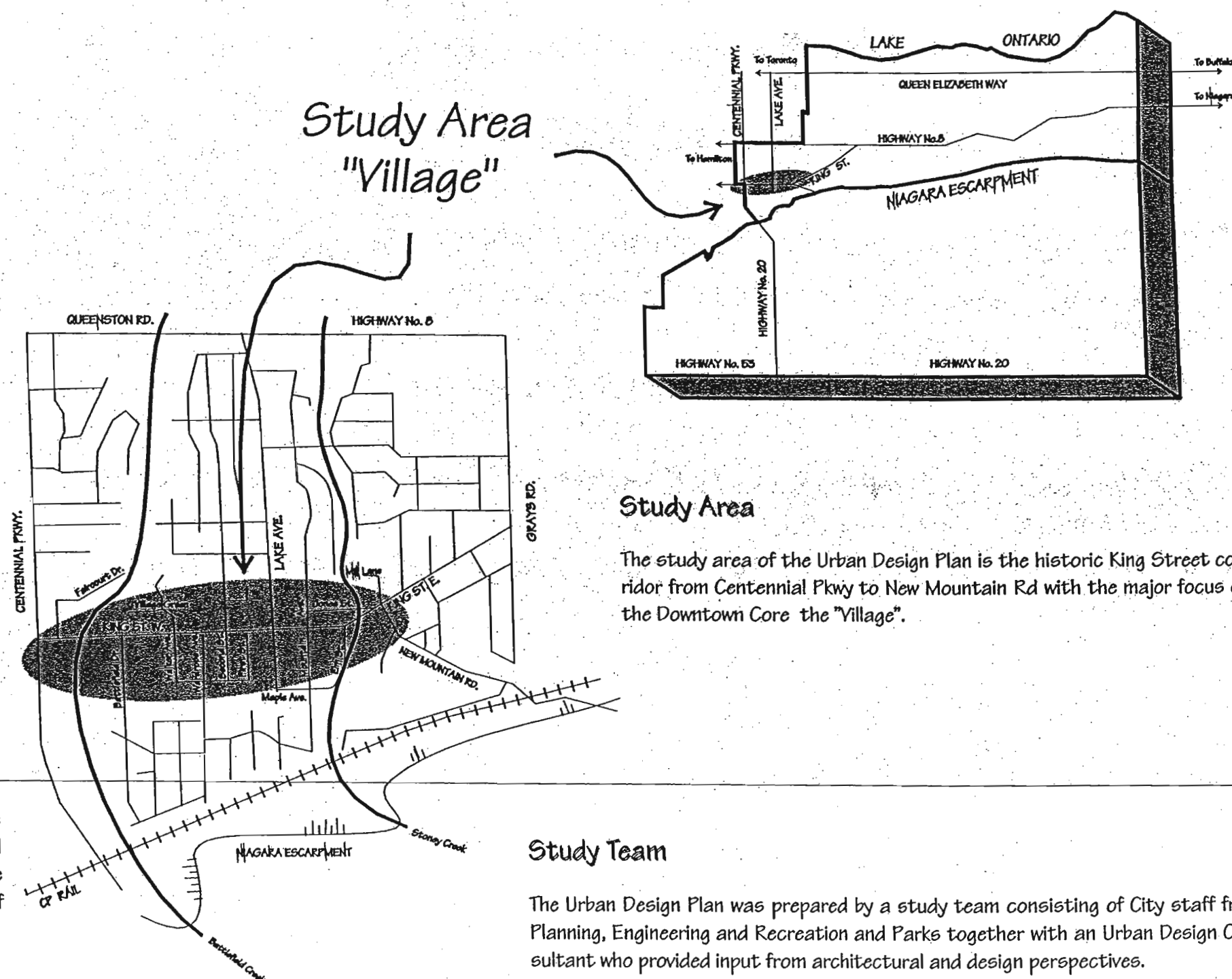
This document is an urban design plan for the City of Stoney Creek's historical business, social and cultural district. It deals with the built-form, enhancement of physical features and landmarks, the streetscape, a sequence public spaces, pedestrianization and the aesthetics of the area in general. It is a vision for the future and is intended to be completed in time for the Bicentennial of the Battle of Stoney Creek (June 2013).

The Urban Design Plan's approach is from two perspectives:

- At the macro level, the overall King Street corridor is evaluated in terms of creating a distinctive downtown that is visually attractive, functional with a strong identity. Opportunities are explored for gateways and enhancing special features such as water courses and historical resources.
- At the micro level, the individual character of places are evaluated in view of their locale within the downtown, built-form characteristics, pedestrian amenities and open space features. Ideas for built form and public space which achieve the stated goals and designs are presented to address a sequential continuity from one place to the next.

Planning Context

In January of 1997, the City Council authorized the preparation of the Olde Town Urban Design Plan. This Plan represents the implementation of the Olde Town Secondary Plan. The Secondary Plan was approved by City Council subsequent to a formal planning exercises. Given the uniqueness of the area, an urban design plan was considered as the most effective method of implementation.



Study Area

The study area of the Urban Design Plan is the historic King Street corridor from Centennial Pkwy to New Mountain Rd with the major focus on the Downtown Core the "Village".

Study Team

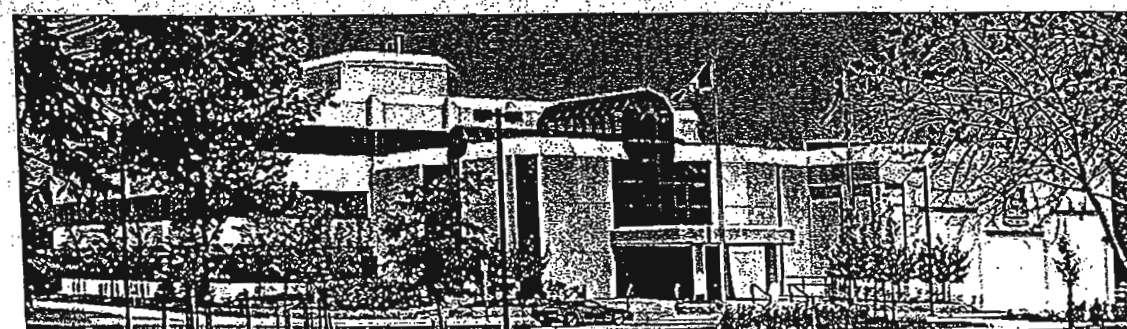
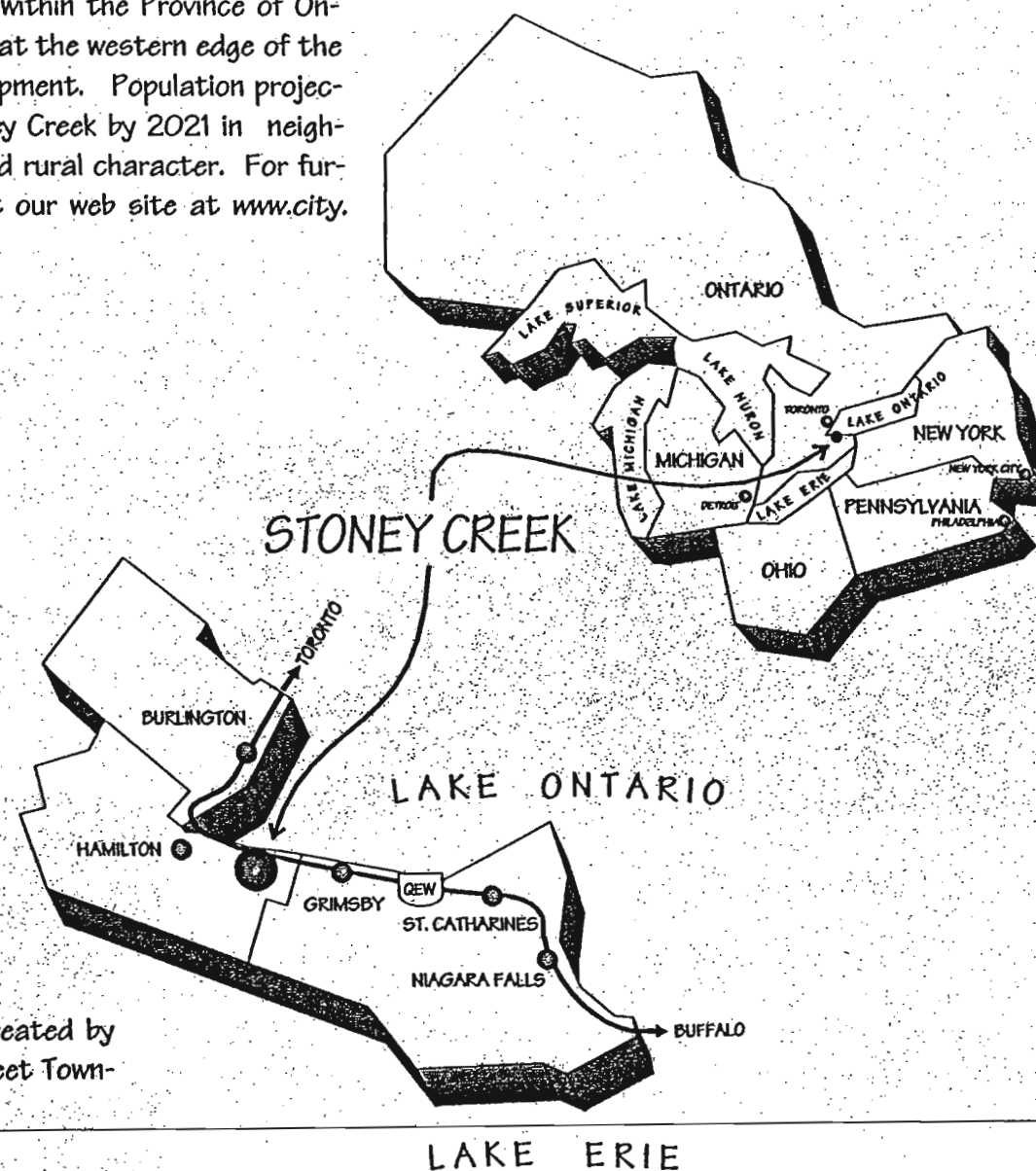
The Urban Design Plan was prepared by a study team consisting of City staff from Planning, Engineering and Recreation and Parks together with an Urban Design Consultant who provided input from architectural and design perspectives.

Contemporary Stoney Creek

Today, the City of Stoney Creek has a population of approximately 55,000 persons and encompasses an area of about 100 sq. km. within the Province of Ontario. It is located on the south shore of Lake Ontario at the western edge of the Niagara Peninsula and is crossed by the Niagara Escarpment. Population projections indicate that 87,000 people will reside in Stoney Creek by 2021 in neighbourhoods that build upon its historical small town and rural character. For further information about the City of Stoney Creek visit our web site at www.city.stoney-creek.on.ca.

The Stoney Creek, originates above the Niagara Escarpment and falls over the escarpment at the "Devil's Punch Bowl", a drop of 30 metres and empties into Lake Ontario. Historically, three mills were located on the creek. A saw mill was located behind the old Hamilton, Grimsby and Beamsville railway power house, on a property known as 27 Jones Street.

The City of Stoney Creek, as a municipality, was created by the merging of the Town of Stoney Creek and Saltfleet Township, both of which had a rich and shared history.



City Hall

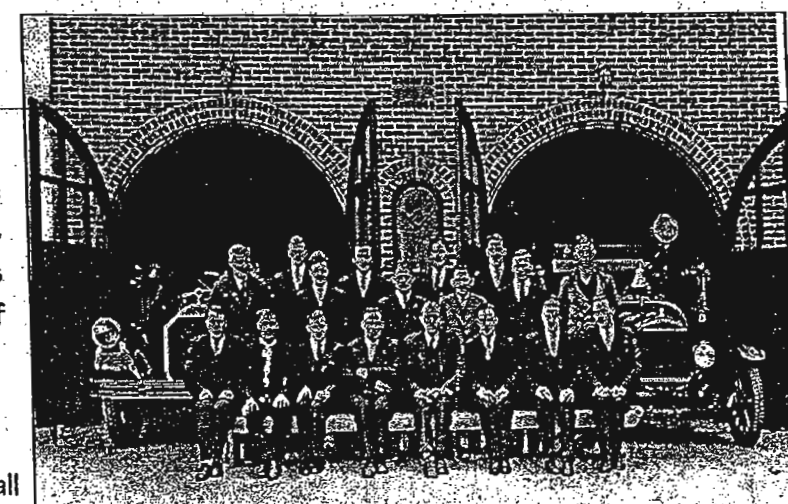
Downtown Stoney Creek and the "Village"

Downtown Stoney Creek has an attractive scenic setting located in the west-central sector of the City in the centre of a mature built-up area. The Niagara Escarpment, located just south of the downtown, exerts a visible presence. The Stoney Creek and Battlefield Creek are also important physical features.

Historically, the "Village" of Stoney Creek served as the central place of Saltfleet Township. Today's downtown is focused principally around this old "Village". In 1853, the County of Wentworth was formed and included the Township of Saltfleet. By 1875, there were two identifiable settlements serving the Township, one was Stoney Creek and the other Winona. Stoney Creek became a Village in 1931 and a Town in 1955, with its Town Hall located on Lake Avenue.

The "Village" served as a focal point for the area. Over the years, it continued to maintain the area's highest population density, retail space and such civic facilities as the former Town Hall, the former Fire Station, Library, community and cultural centres, parks and the former Post Office.

The civic use with the most effect on, and interest to the greatest number of citizens, was the Town Hall. In 1974, when Stoney Creek and Saltfleet were amalgamated, the Town Hall was relocated and as a consequence, a certain degree of civic attention was withdrawn from the downtown. Another factor that contributed to a loss of focus on the "Village" was the shift of retail functions to new commercial areas, including, large shopping malls. In spite of these changes, major civic events continue to take place in the downtown and other civic-oriented organizations which remain or have recently located there all contribute to a genuine sense of community which must be nurtured in order to preserve and enhance its historical role as the centre of Stoney Creek.



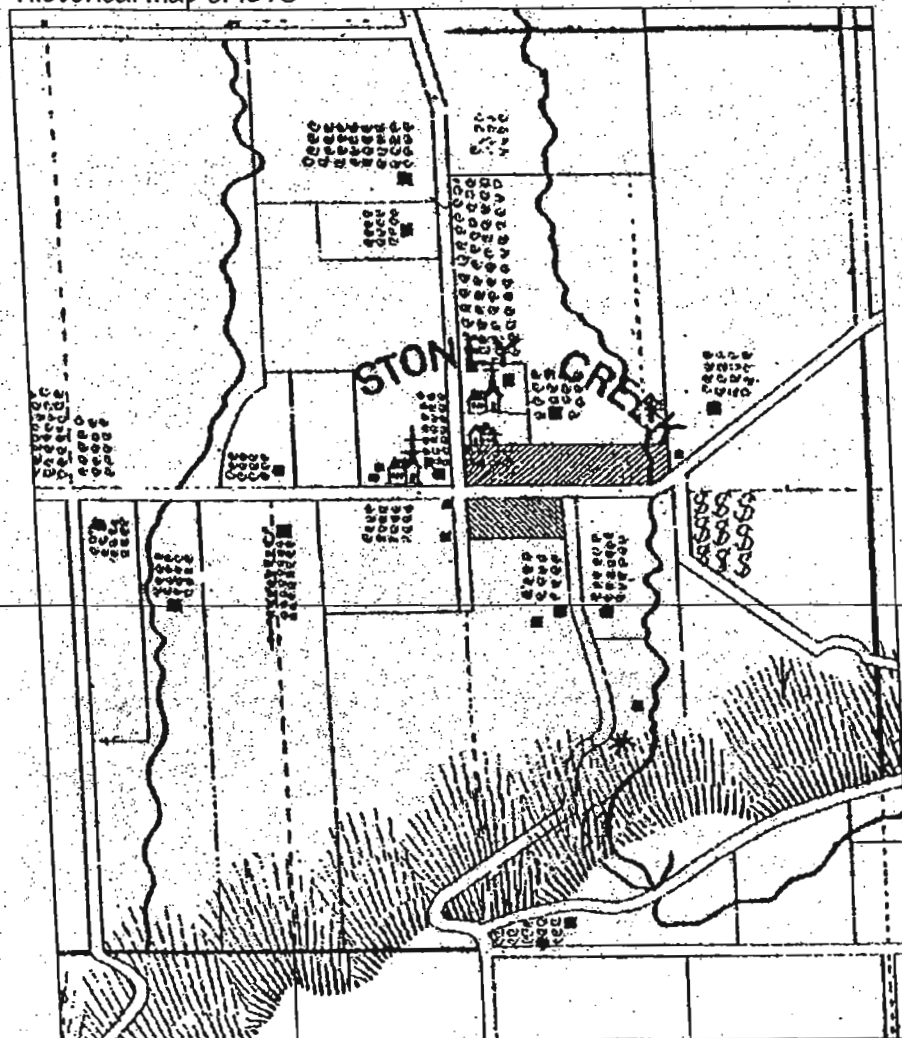
Old Fire Hall & Town Hall

First Settlement

The first settlers arrived to the area around 1786 and it was first surveyed in 1788. Stoney Creek was a stopping place on the winding Indian Trail along the "foot of the mountain" serving as the main road from Niagara. This stopping place was popular in the 1790s with a number of log houses, taverns, a store, a church and a saw mill. Today, the trail is known as King Street and it remains an important route.

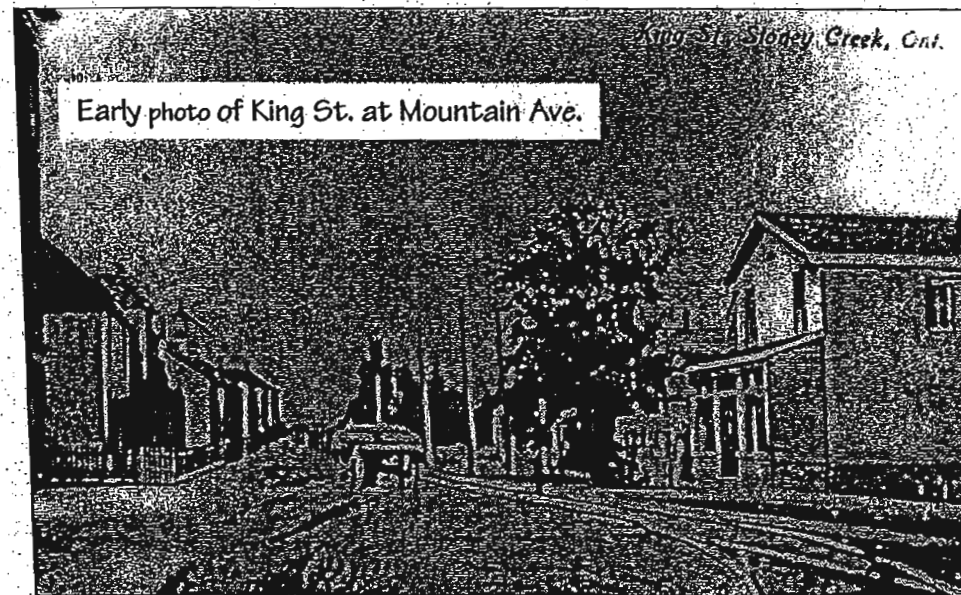
In 1813, this little hamlet was to become the scene of one of the most important battles of the War of 1812 and to become an immortal name in the history of our country.

Historical Map c. 1875



Naming of Stoney Creek

Local historians have debated the naming of Stoney Creek. One theory is that it was named after the creek and the creek was named because of the stones in the bottom. This theory was put forward as early as 1796 by Mrs. John Graves Simcoe. However, early inhabitants claim that it was named after an early resident of the area, a mysterious Mr. Stoney.



Battle of Stoney Creek, War of 1812

On June 5, 1813, an American force of 2,000 to 3,000 intended for York (Toronto) marched from Forty-Mile Creek (Grimsby) encamping near what is now Battlefield House. About 2 a.m. on June 6, 1813, a company of 704 British regulars under the leadership of Colonel Harvey, engaged the American forces in a surprise encounter.

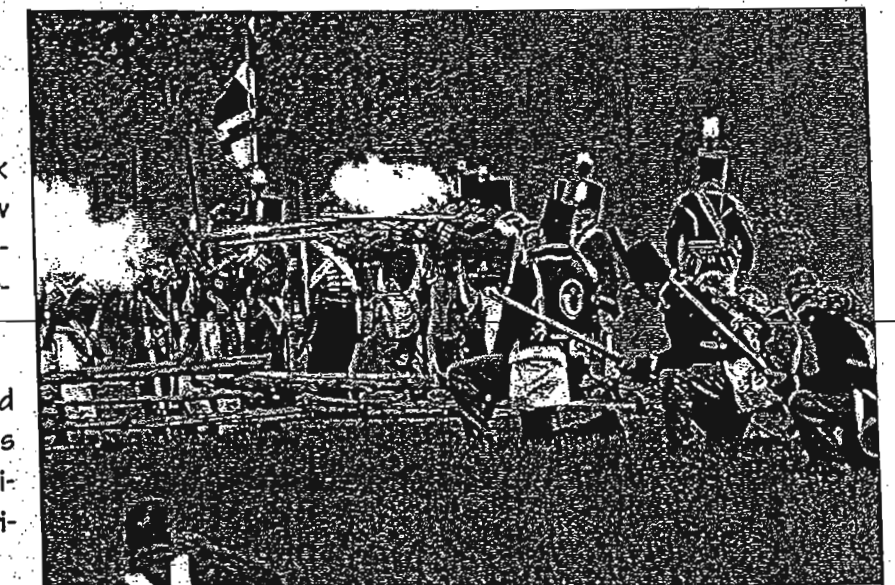
As the American field guns were brought into action, a force of regulars charged the cannons and put them out of action and the two American brigadier-generals were taken prisoner. Later that day, after the American forces retreated to Niagara, the dead of both sides were buried. There were 216 British and 175 American casualties. At least 61 men from both sides were killed in battle.

The Battle of Stoney Creek is regarded as a significant turning point in the War of 1812 as it marks the furthest extent of American advance into Upper Canada during the conflict. The Battle is commemorated annually at Battlefield Park during which a re-enactment is held.

The Mysterious Mr. Stoney

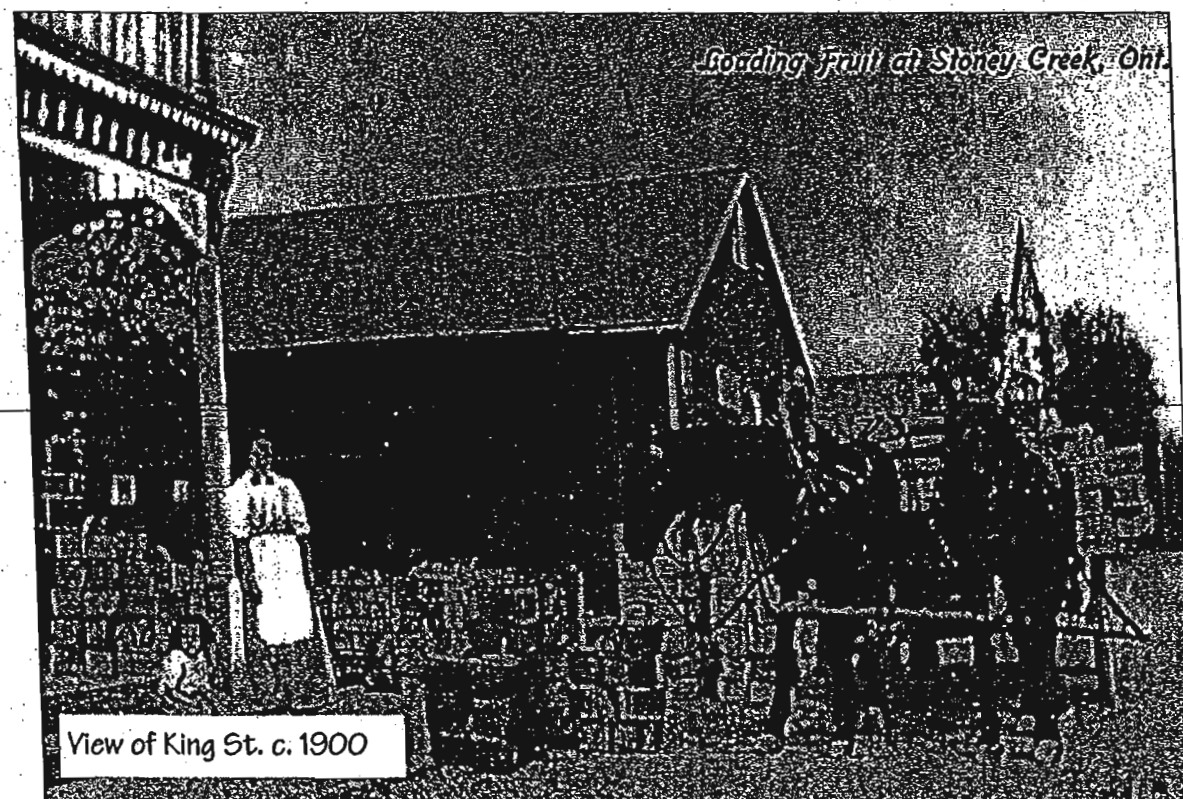
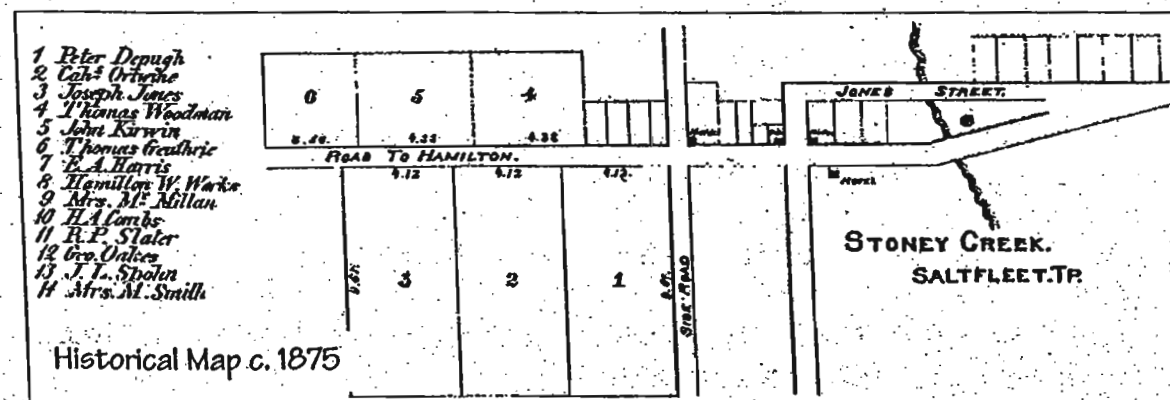
Jim Stoney, from whom Stoney Creek may have derived its name, was a trapper and hunter who lived along the edge of the creek. Proof of his existence is in a school teacher's diary entry in 1816 which states: "We all went back to the mountain to one of Jim Stoney's trapping huts." It is alleged that Jim Stoney lived along the stream before the first settlers came to the area. It may simply have been assumed that the creek was named after the stones. However, there were trappers in the area before 1796, and it is probable that a trapper's hut existed along Stoney Creek. It is possible that the first people through the area referred to the stream as Stoney's Creek, and that it was later shortened to Stoney Creek. Since this stream is no more stony than other streams in the area, the early settlers would probably have needed a more important reason for giving it a special name. The other creeks were named simply by their distance from Niagara so that the traveler would pass Twelve-Mile Creek, Twenty-Mile Creek, Forty-Mile Creek and so on. That Stoney Creek received a distinctive title instead of being referred to as Sixty-Mile Creek, seems to indicate the settlement was named after its first citizen, Jim Stoney, trapper.

Annual re-enactment of Battle of Stoney Creek



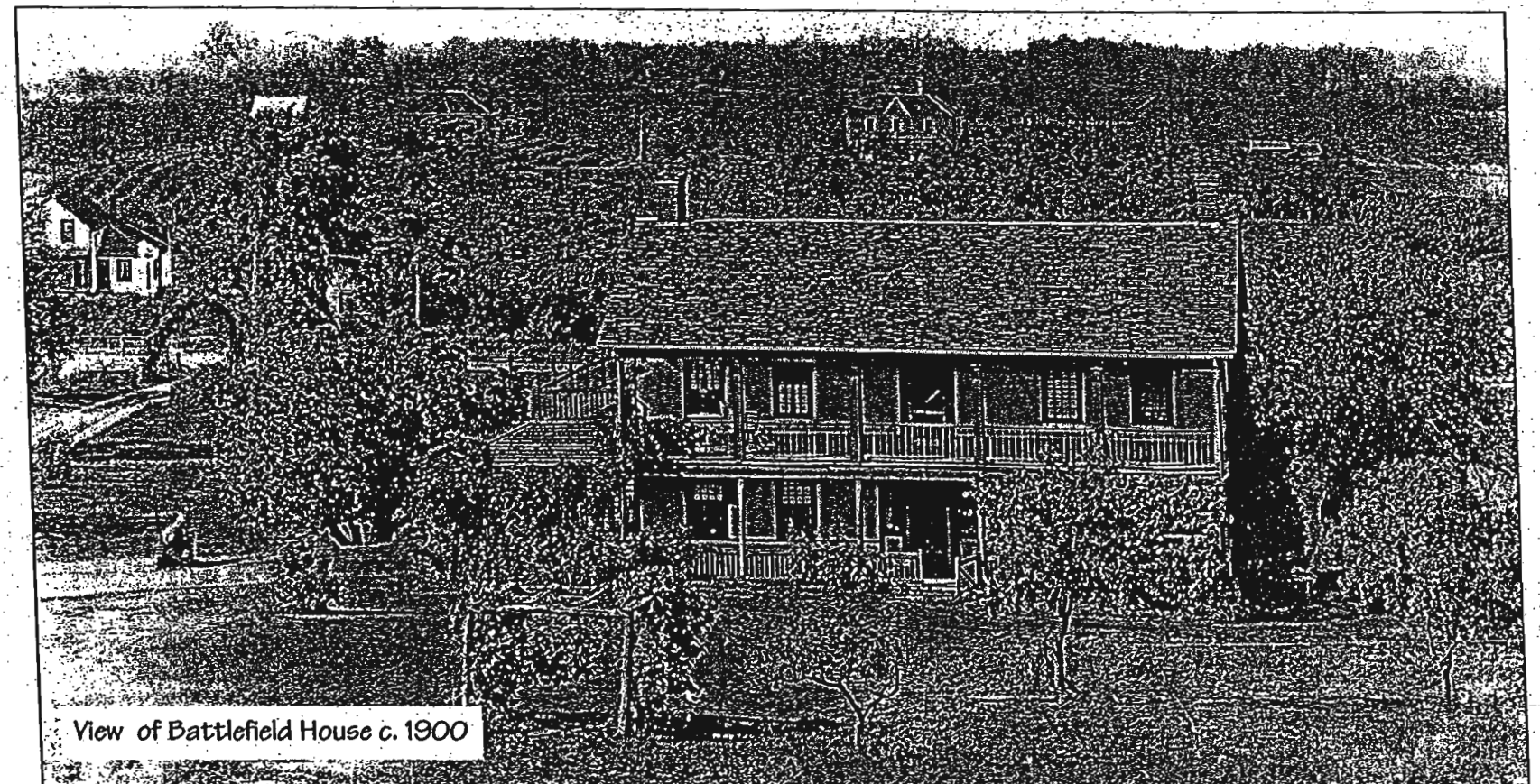
Augustus Jones

Augustus Jones was the famous surveyor of Upper Canada during the late 1700's and early 1800s who played an important role in the settlement of the province and in the Indian Treaties. Jones and his immediate family made their home in the Stoney Creek area, in fact one sister lived in Battlefield House at the time of the conflict. Jones resided along the Stoney Creek where his family built a saw mill just north of the present Jones Street near Mill Lane. His house and barn were often occupied as barracks during the 1812 hostilities. His wives and best friends were Indians and he lived in the two worlds simultaneously and served as a bridge between both in the early years of Upper Canada. Jones died in the Brantford area in 1836 having left the Stoney Creek in 1816.



Early Economy

Stoney Creek was a busy "business area" for most of the 1800s. Stage coach inns were the first private enterprise in the area; the grain and saw mills along the creek were a close second. From about 1840 to 1853 the growth of the Village became marked. It was the municipal center and also became the seat of all the mercantile business transacted in the area. In 1847 the first two grain storehouses in the county were established. Since Hamilton had no storehouses, Stoney Creek became the head of navigation on Lake Ontario. In 1853 the storehouses were moved to Hamilton. In the meantime, the first organization of the municipal council of the township took place at Stoney Creek, on January 21, 1850.



Women's Institute

On the night of February 19th, 1897, Adelaide Hoodless visited Stoney Creek when the first Women's Institute meeting was held at Squires Hall (4-6 King Street East) to explain her idea of forming a world-wide association of women. The original constitution was drawn up at a home in the area. The Institute has been called "Canada's greatest contribution to the world".

"Sequence of Public Spaces"

The vision of the Urban Design Plan is to enhance the existing physical characteristics and historical landmarks of the area together with urban design interventions to create a "sense of place". The approach is to create a sense of place by a sequence of public spaces unified by a harmonious streetscape. The sequence includes existing public spaces as well as new spaces and their connection to each other. The streetscape will also be enhanced so that it serves as the connecting element which gives relation to the public spaces.

Identifiable public spaces have the capacity to enhance our lives. They impart a pleasing sense of comfort and security that is as important to the enjoyment of life as sunlight and a place to rest. They are a basic component of good urban design.

When entering a new place, people scan the parameters of the space, pausing to study features of interest. If the design features are too simple, they may not broder with a more careful examination. People enjoy reexamining familiar areas where multiple levels of visual enjoyment are offered and where there always seem to be new visual relationships and effects to appreciate.

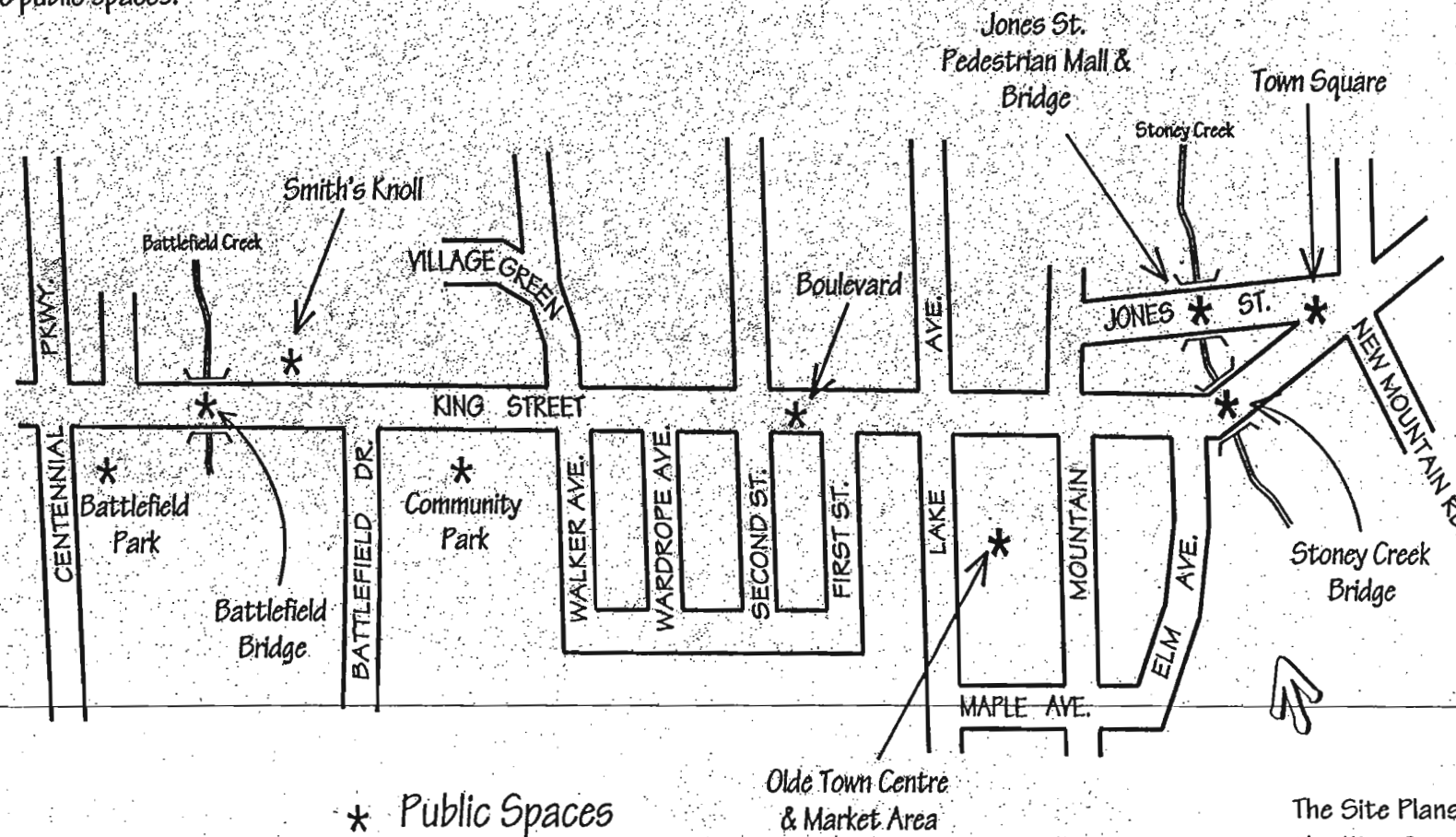
Public open spaces or outdoor squares do not have ceilings to define the height of the space within their walls, and a street has only two walls with which to define its space. If enclosures do not exist, or are low, views outward are not contained enough to provide a sense of place. Holding space in place requires strong enclosures that "hold" space. This is accomplished with buildings, sculptural projections (arcades), landscaping and textures.

The shape of a square or public space should permit it to be experienced, in its entirety, from any point within. Simplicity of form, so one can be easily comprehended, is also desirable. A form that facilitates the easy assembly of the total impression, such as a simple easily recognizable shape, is effective.

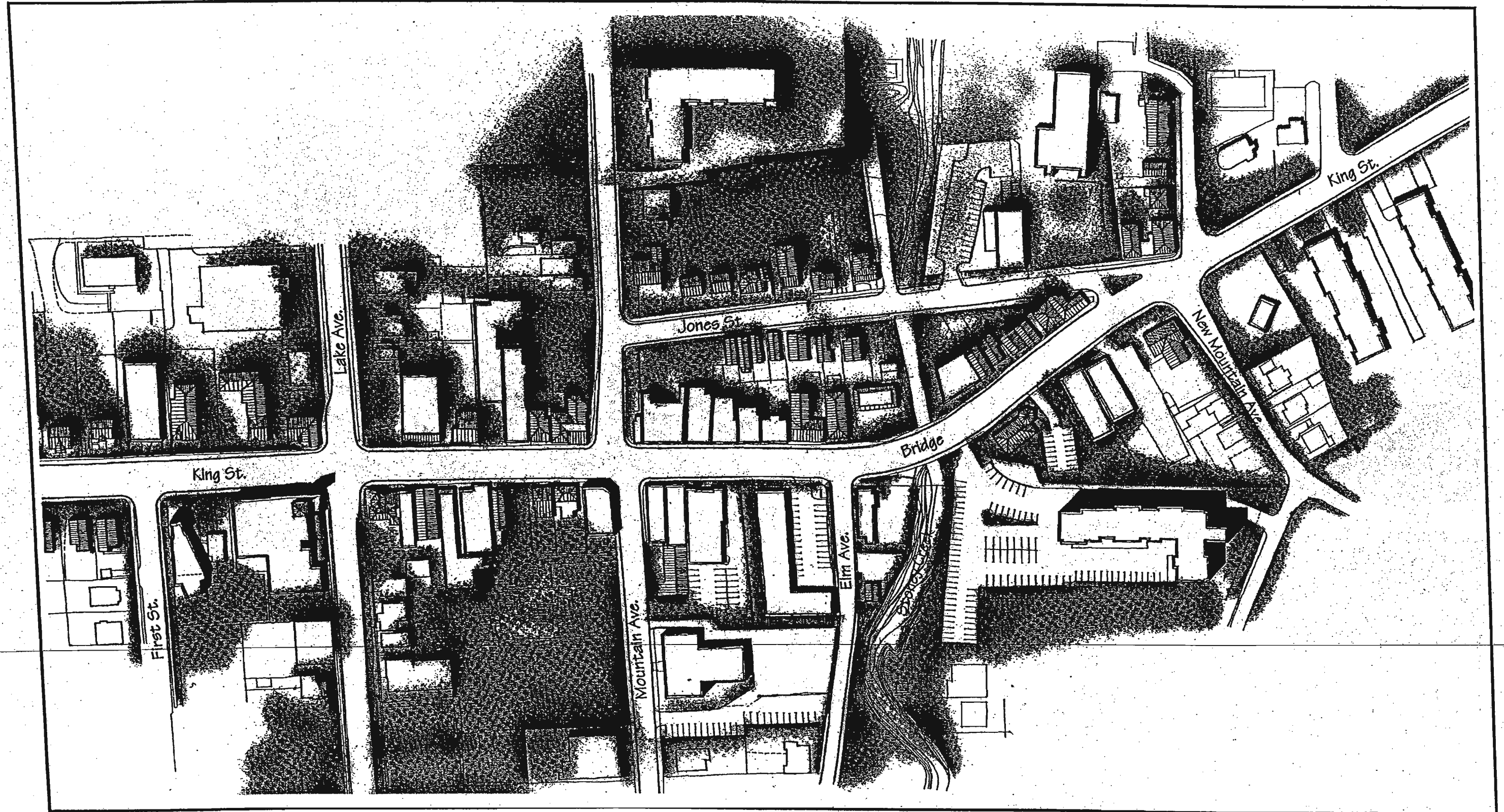
Where the enclosure is strong, then a flat floor that offers minimum impedance is desirable. If the enclosure is weak or non existent, then the careful sculpturing of the floor can be used as a counter balance. Dropping the floor down a few steps may introduce an extra set of defining lines.

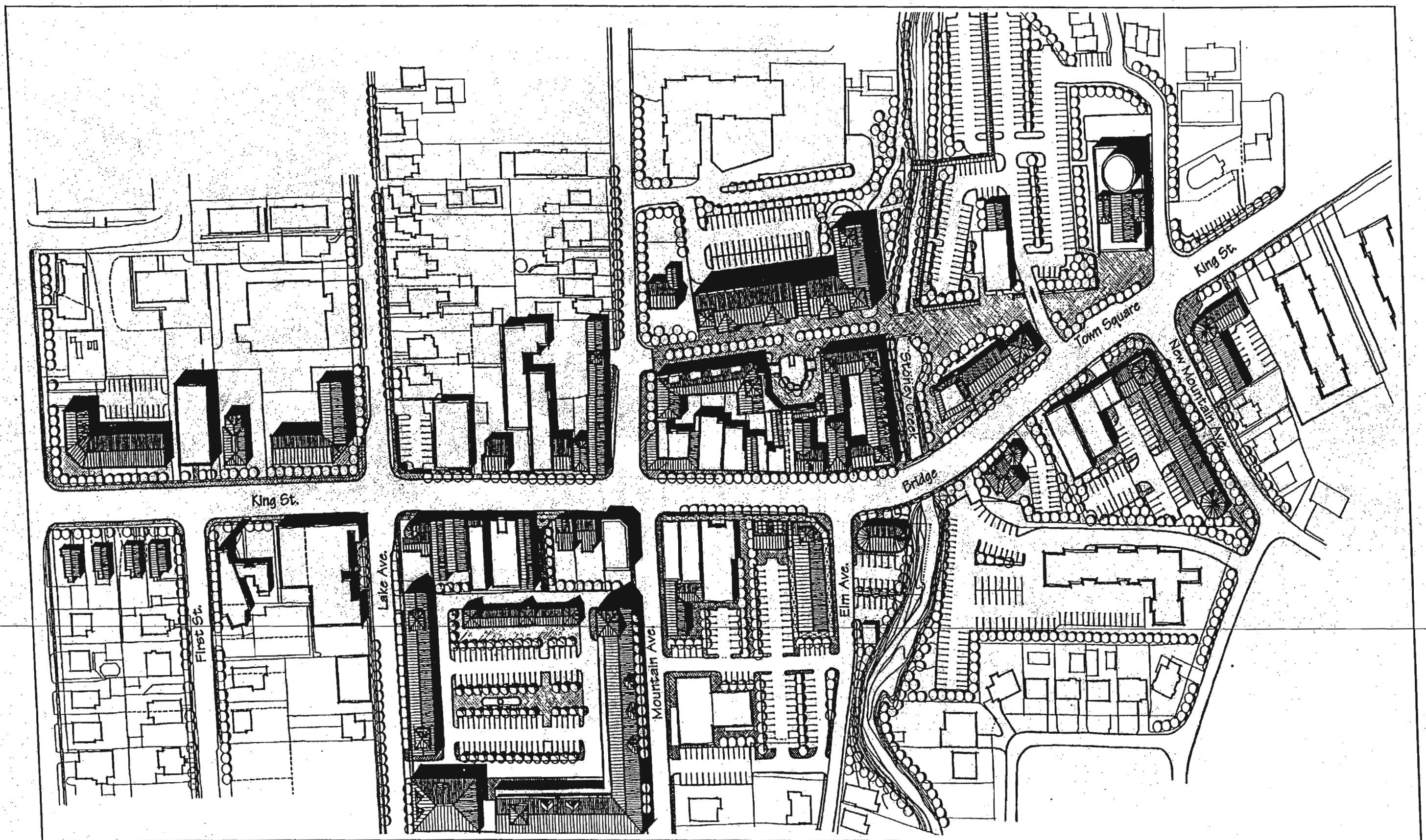
Clarity of form and closure are weakened when the plaza frame is affected by roadways or other openings. The streets that abut the space may invade its floor, and if the traffic is heavy, it will disturb the perception of the frame with vehicles. Whenever possible, a strong, identifiable separation is required.

The spaces and landmarks proposed by this plan range from formal places to bridges, open spaces, the roadway and the streetscape. The above-noted principles were used in the design of each space in this plan. These concepts are discussed in more detail in Section 5.0 of this Plan.



The Site Plans on the following two pages identify the "Core Area" which has been expanded to include the King Street corridor between Second Street and New Mountain Road. This area represents the historical downtown or "Village". The Existing Site Plan identifies the present lot and building fabric, while the Future Site Plan provides an indication of how the the area may redevelop in accordance with the vision and principles contained in this document. The architectural and design concepts suggested are intended to be a guide and to direct redevelopment so that it addresses, in a comprehensive manner, the future built-form.





The Streetscape

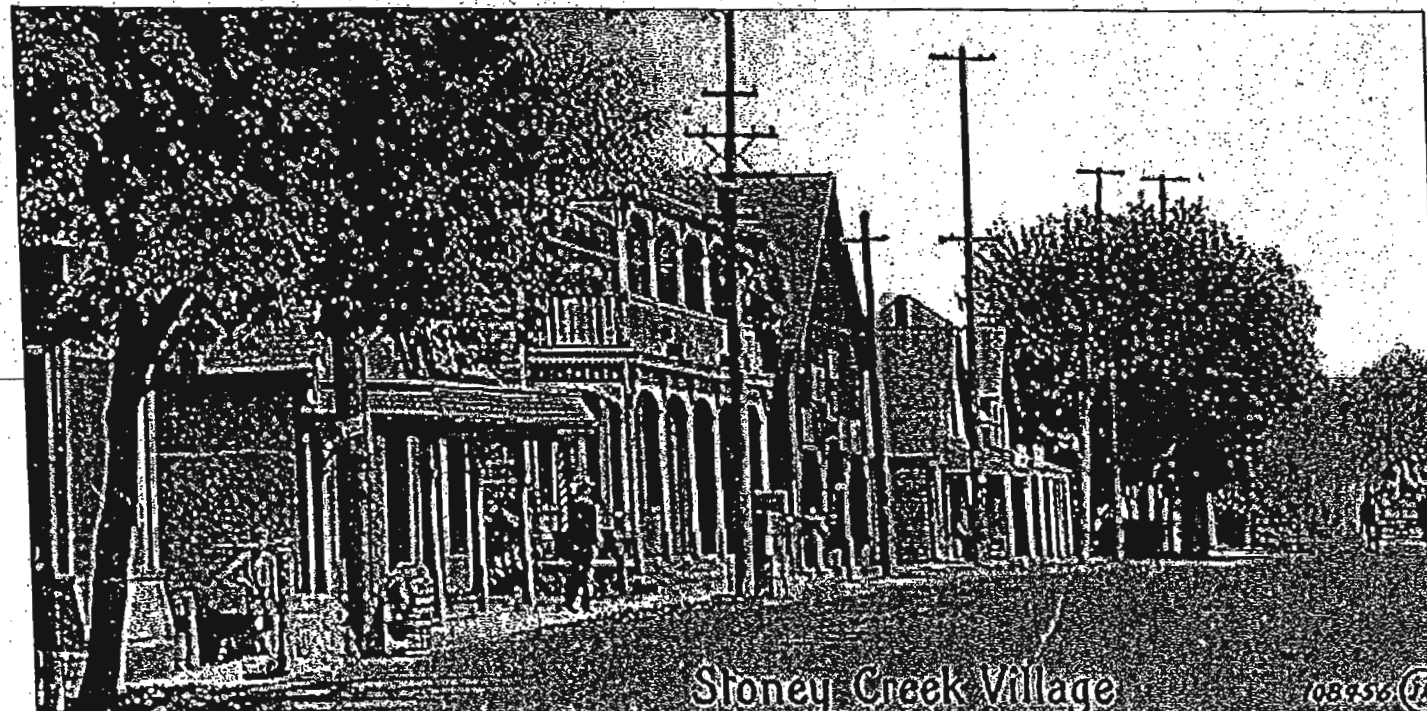
The streetscape is composed of a number of unifying elements. For the purpose of this Plan, the following elements considered relevant are:

- Design Theme
- Sidewalks & Crosswalks
- Landscaping & Planting
- Facades & Signage
- On-Street Parking
- Lighting & Utility Wires
- Furnishings & Amenities

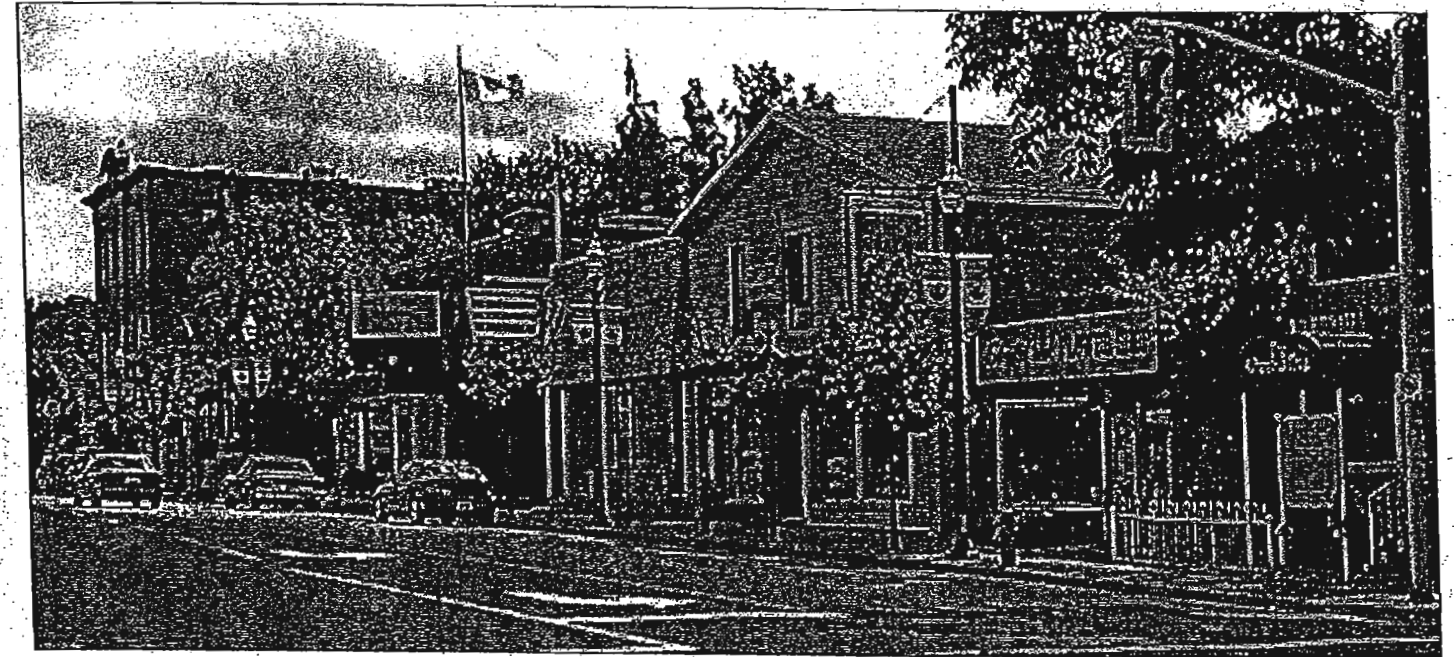
The design of all the streetscape elements must be coordinated so that the area is perceived as a cohesive and attractive place for pedestrians and for persons travelling through it. It should have a distinctive "look".

Vision

It is important to realize that the role, function as well as the aesthetics of King Street are envisioned to be quite different from which currently exist. Essentially the vision for King Street is that it return to its historical role of a "small town" downtown street. To achieve this, a number of improvements and modifications are required to the streetscape. These improvements, together with the sequence of public spaces, are intended to slow traffic so that the area becomes more pedestrian friendly.



View of historical "Village" streetscape, north side of King Street, east of Lake Ave.



View of south side of King St., east Lake Ave.

The streetscape is a public space. It is concerned with the treatment of the sidewalk, roadway, landscaping, or other areas located between buildings. The goal of this plan is to unify the public spaces and to maximize its value as an urban amenity while addressing issues such as safe and adequate movement, sunlight and protective measures from local weather conditions. The buildings that create the space may seem secondary; however, a major influence on the streetscape is the building facade which has a major unifying influence to the character of the street presence. Existing and new facades should reinforce streetscape unity with a continuous street. This means reinforcing the public space which is within the street right-of-way.

The major design influences springing from a review of building facades include maintaining a continuity of pedestrian front entry to buildings from King Street. Existing entrances should be preserved or replaced with redevelopment, and new development should create new street level entrances. Where this is not likely to occur soon, the voids should be reinforced with landscape screens.

Typical Streetscape Treatment

The "Typical" streetscape treatment standard will be used most often along King Street and it is the basis for the other treatments. It is characterized by trees, paving and lighting. Specific ideas regarding the placement of elements are discussed, but in general the following principles must be observed:

- A rhythmic 10 metre module to coordinate placement of trees, lamps and paving patterns. This pattern is to be observed rigorously, and adjustment only if essential. These are the primary elements and have the greatest effect on the streetscape as a whole.
- As pedestrian movement space on sidewalks is minimal, furnishings such as benches, planters, bollards, bicycle racks, telephone booths and fountains should not be located in this type of treatment.
- Crosswalks at intersections must be identified with a brick pavers and a concrete paving system to enhance the aesthetic quality of the area, to slow traffic and to provide greater pedestrian safety.



King St. at Lake Ave.

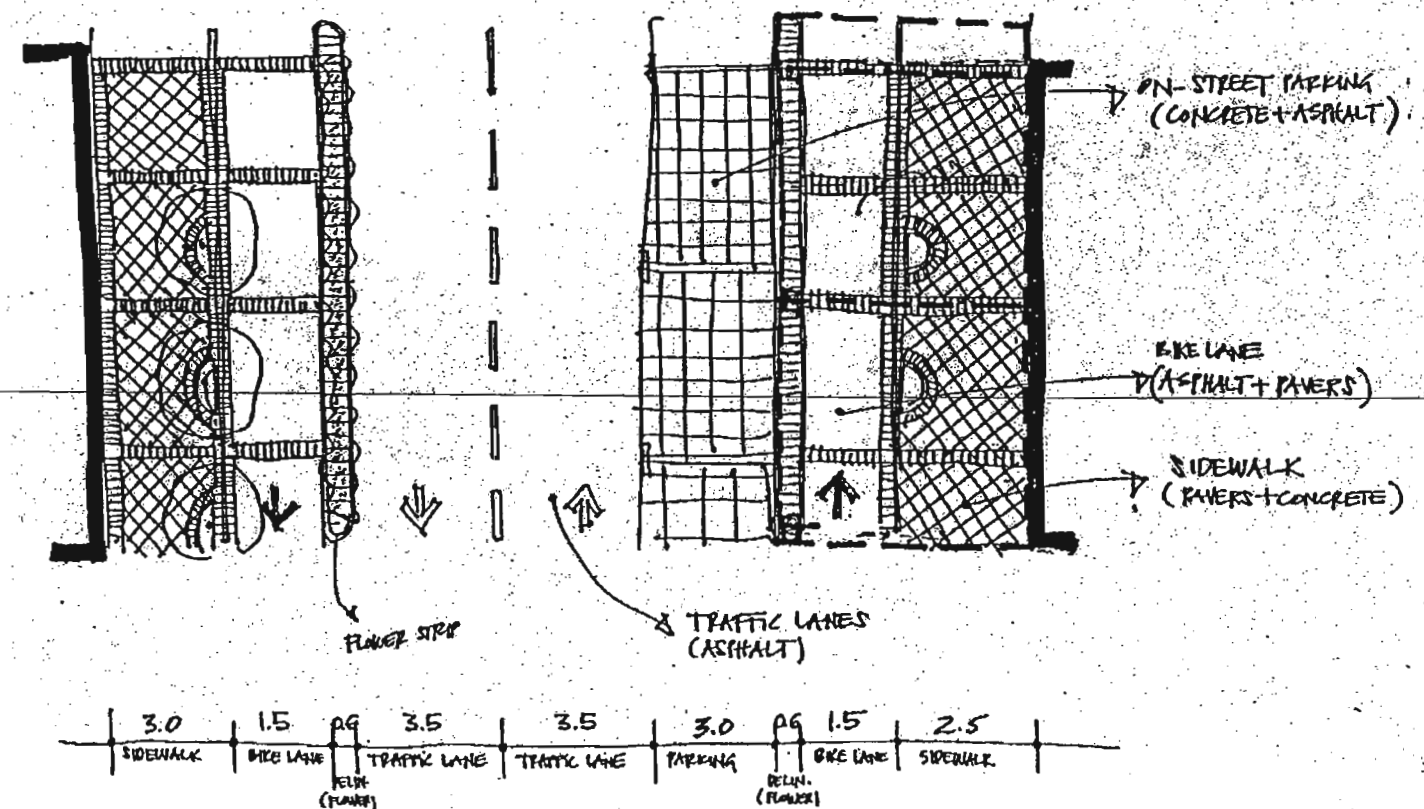
Special Streetscape Treatment

The "Special" streetscape treatment standard is intended to be used to enhance public spaces and special areas. Its main function is to provide more pedestrian space by widening sidewalks into the vehicular space where this is not required and to enhance the aesthetic image of the area with landmarks.

Since typical treatment sidewalks do not have the space for many furnishings, these areas allow the use of benches and planters for seating clusters, as well as space for kiosks, phone booths, newspaper boxes and other streetscape amenities.

Under most circumstances, the module technique of the typical treatment is utilized, but in a more intense fashion. In some cases, such as the Jones Street Pedestrian Mall and Jones Street Bridge, the pattern may change to integrate better with the design of Town Square at the King Street- New Mountain Road delta. This type of modification is encouraged, provided that the same palate of materials is used.

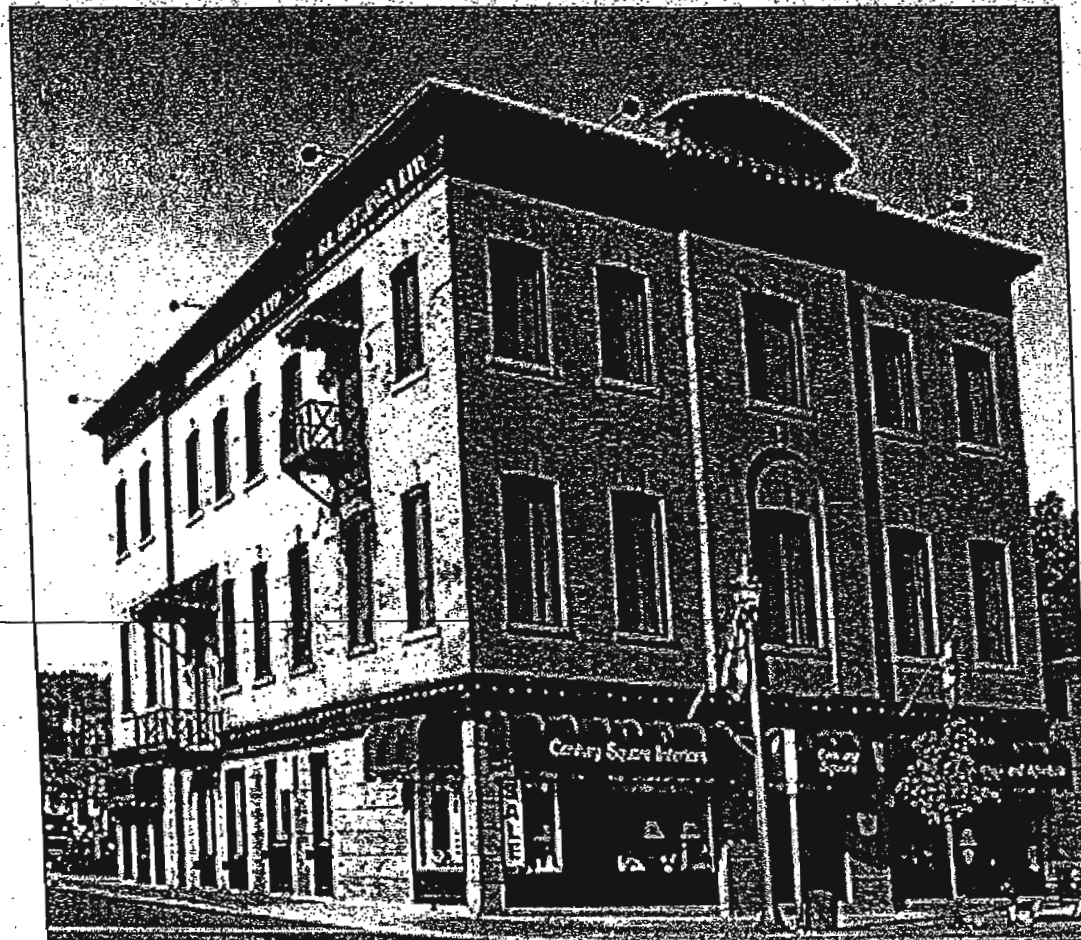
Conceptual roadway delineation for downtown core



An essential requirement in developing a unified "look" to the area is to establish a design theme. The theme should reinforce the best of what already exists. As a unifying force, the streetscape should retain a design integrity of its own. The design of each streetscape element is conditioned by expression, in a contemporary way, of the historic sense of order and use of materials.

The King Street corridor under consideration displays a mix of styles reflecting the historical development and land use patterns. There are some buildings with good architectural character while others have no regard for the local condition.

For the purpose of this analysis, the area has been divided into areas which display distinct elements. They are the Downtown Core, King Street West, Jones Street and Downtown South.



The historic Institute Building at 32 King Street East, built 1901

Example of appropriate scale and vernacular for the Downtown Core.



Downtown Core

The existing downtown core extends from Lake Ave to the Stoney Creek. As part of this Plan, the Core is to be extended easterly to New Mountain Rd and westerly to Second St.

Heritage Features

This section of King Street is the "urban heart" of historical Stoney Creek. It includes a number of heritage features, including the Institute Building and properties at 4-6 King Street East, which have been listed as being of "potential historical and/or architectural value". Other buildings and landscape features (such as Stoney Creek itself) also contribute significantly to the special "heritage" urban identity.

Built Form

The built-form of this section of King Street is characterized by the relatively fine scale of diverse individual buildings ranging in age up to a century or more which cluster quite consistently close to the street line creating a compact urban center. Heights vary from 1 to 3 storeys, but the majority of facades are 2 storeys in height. Broadly spaced, the existing heights give spatial definition to the street; however, the pattern is broken on a number of occasions by more recent developments at the north-east corner of King Street East and Lake Avenue and at the south-west corner of King Street East and Elm Avenue. In both cases, parking areas open to King Street have weakened the distinct urban character of the area by creating gaps in the streetscape. Only a few of the existing properties have been restored, and signage varies widely.

Design Themes

It is appropriate that the design theme focus on the heritage character of the Core. The gateway features, such as the Town Square at the King Street - New Mountain Road delta and the Boulevard on King Street at Second Street, will denote the limits of the downtown and provide a stronger visual character. An embellished bridge over Stoney Creek will form a centerpiece to the enhanced streetscape of the Core. The gateway features and bridge embellishments are appropriate vehicles for the elaboration of the theme.

Jones Street

Jones Street is a very interesting and distinctive enclave on the edge of the downtown core, running just one block from Mountain Avenue to New Mountain Road delta. It is composed primarily of wood-frame single detached dwellings. The Stoney Creek crosses Jones Street near the delta.

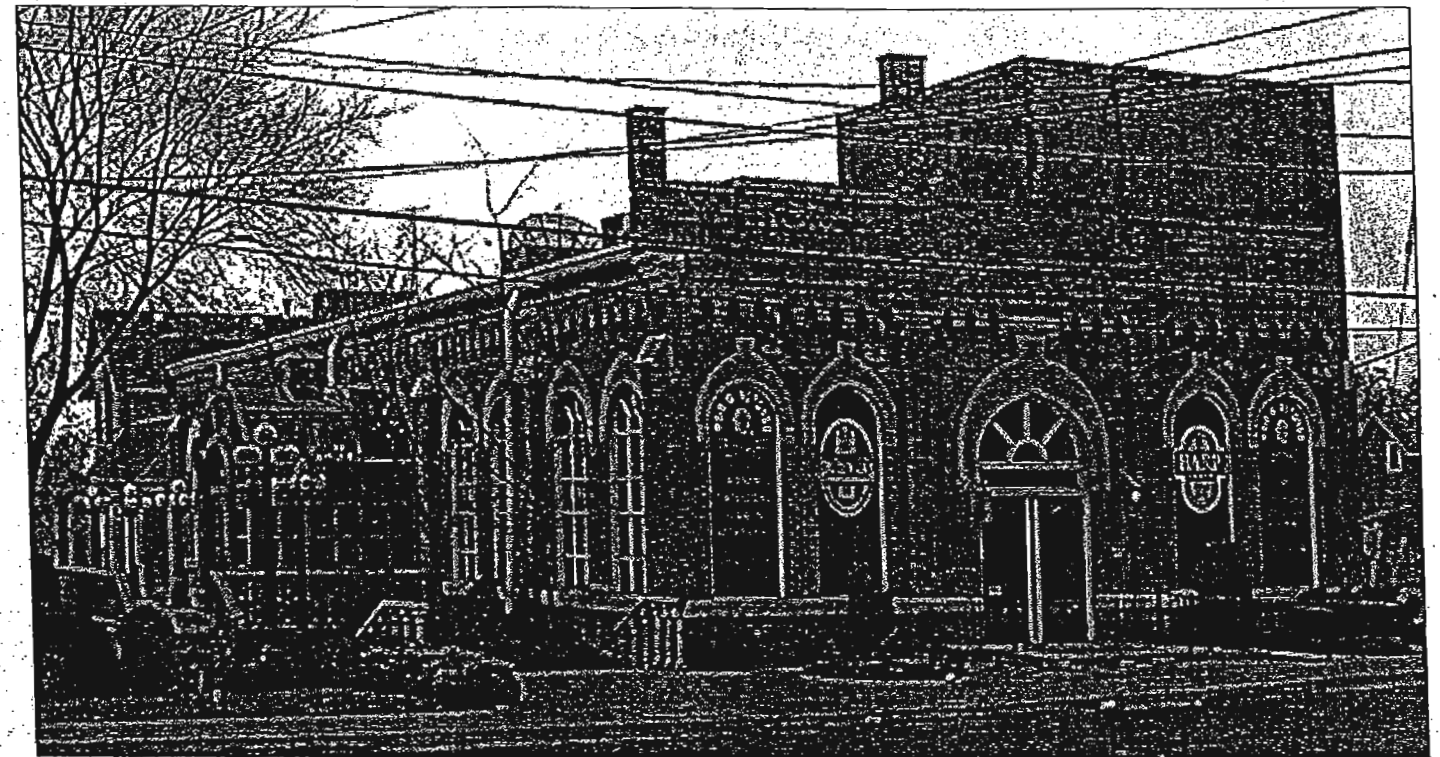
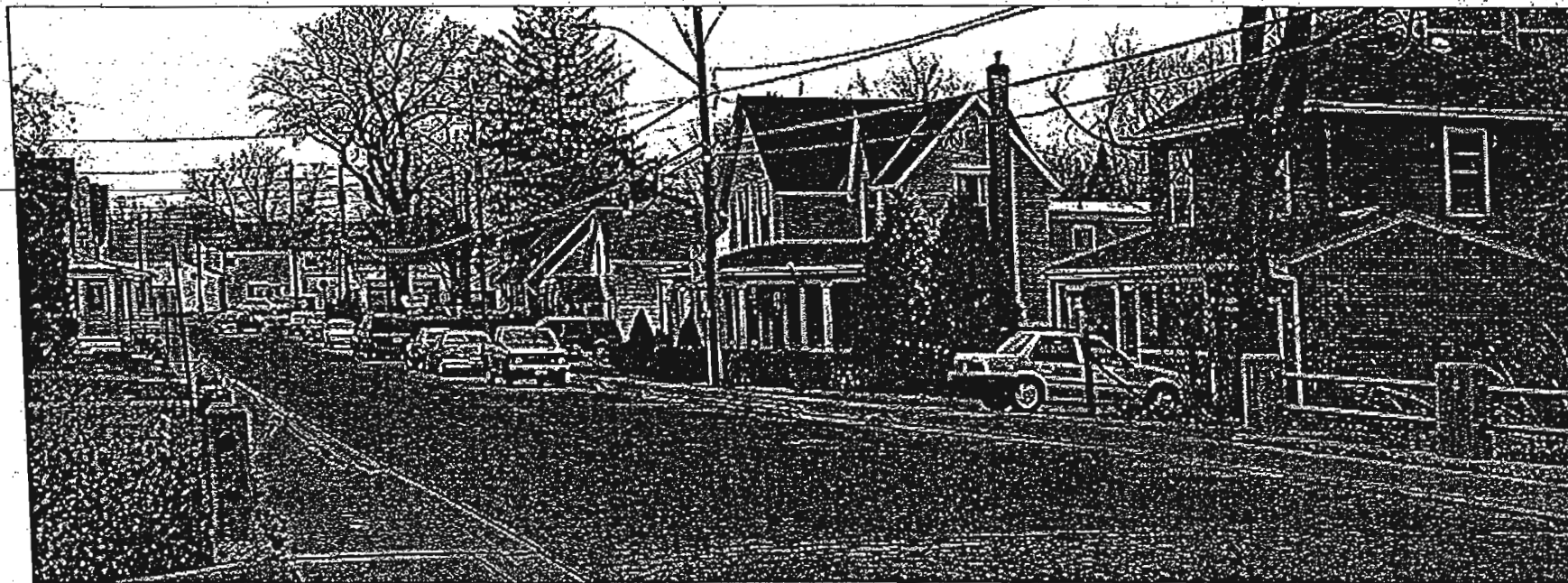
Heritage Features

Jones Street, according to tradition, is named after Augustus Jones where his family had a saw mill on the site of 27 Jones Street. A dwelling at 1 Jones Street ("Locust Lawn") is listed as a potential heritage property but requires extensive restoration. Some of the dwellings on the south side have been appropriately restored. The former H.G. & B. Railway Powerhouse has been restored and converted to an appropriate use. The remaining properties along Jones Street are developed with frame single detached dwellings which contribute to a consistent and distinctive streetscape, especially on the south side of the street, from Mountain Avenue to the Stoney Creek.

Built-Form

The pattern of residential buildings from Mountain Avenue to the Stoney Creek is tight to the street on the south side, and consistently set back on the north side. The exception to this rule is the parking lot on the south side next to Mountain Avenue. This gap compromises the streetscape somewhat and as a result, a landscaping screen along the Jones Street frontage would be desirable. The buildings are characteristically two storeys, and form a quite convenient spatial grouping. The facades are in relatively good condition and, since the street remains primarily residential, signage is not an issue.

View of north side of Jones St., from Bridge to Mountain Ave.



The former H.G. & B. Railway Powerhouse at 21 Jones St. converted to a restaurant/



Historical property at 1 Jones St., "Locust Lawn", in need of restoration

Possible Design Theme

Jones Street calls for a complementary theme similar to King Street but more contemporary. Because of its location it would be desirable to explore pedestrianization in a formal way. Necessary are linkages to King Street along the Creek and a mid block public space. Enhancement of the Jones Street Bridge and the creation of a Town Square at the delta will add to the area's character. Any new development on the north side should be comprehensive, approx. 4-5 storeys and mixed use in nature. The south side has good potential as a retail/office area.

King Street West

This section stretches along King Street from the Downtown Core to Centennial Parkway. King Street West is more contemporary than the Core and does not have the strong street line definition visible in the downtown core. However, this area has very strong historical connection, particularly related to the Battle of Stoney Creek.

Heritage Features

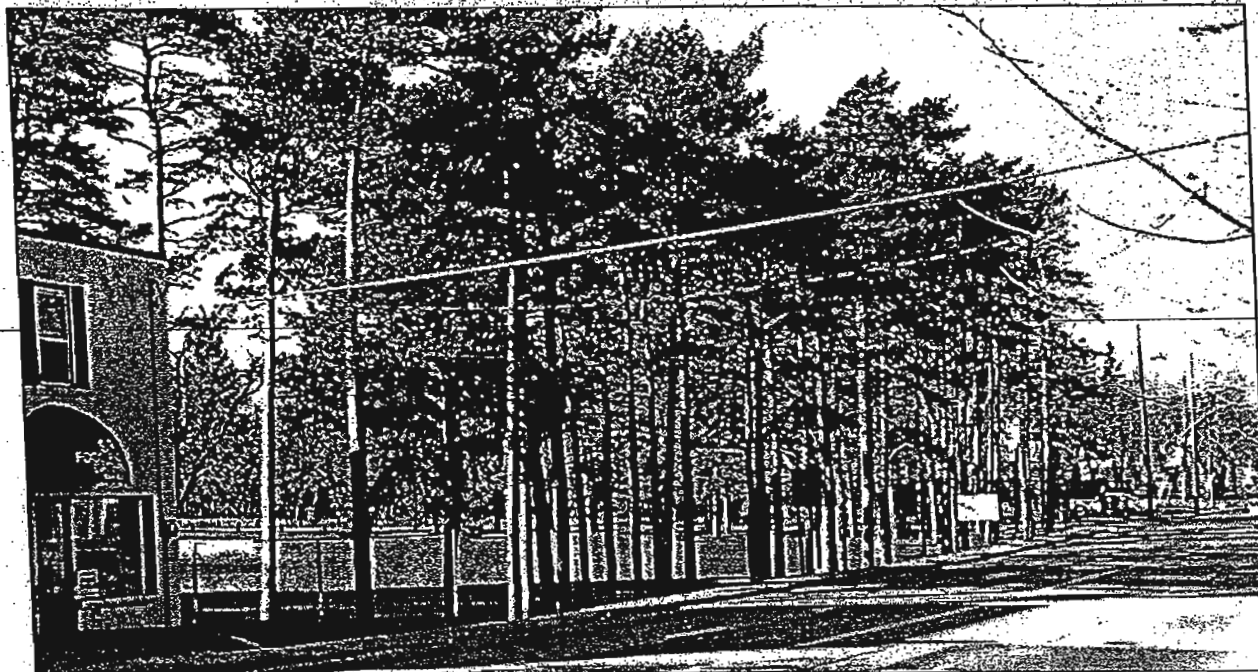
The most significant heritage landmarks are Smith's Knoll Memorial and Battlefield House, Monument and Park. This last is of course, a major historic site and contributes significantly to the character of King Street and identity of the City.

Built-Form

The built-form on this stretch of King Street is somewhat problematic in urban design terms, as there is little thematic continuity. There are a mix of "house form" commercial uses, apartment buildings and newer commercial buildings. Generally speaking, buildings are set back, thereby providing a logical basis along with land use for distinguishing this section of King Street from the core. Yet the setbacks themselves vary as do building heights and forms. Thus, despite the presence of Battlefield Park, this section of King Street lacks clear built-form definition and signage is more discrete than the core.

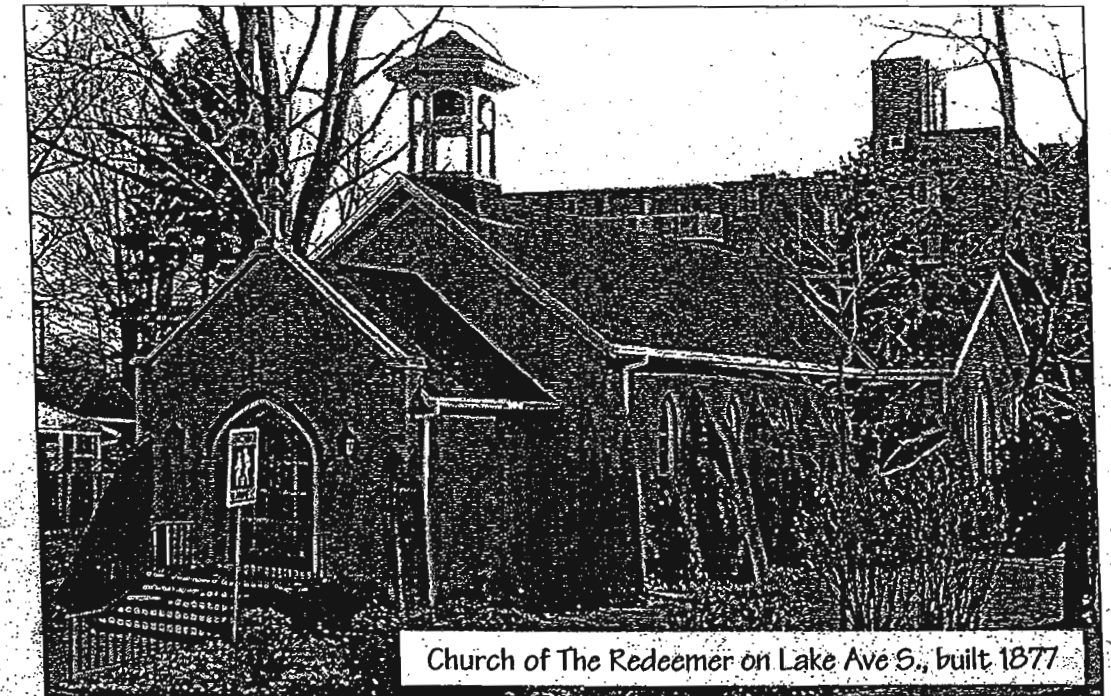
Design Themes

The design theme which would most enhance this area would be a planting and landscape theme complementary to the existing built-form and land-uses, and springing from the strong open space image of Battlefield Park. The Battlefield Creek Bridge and Smith's Knoll should become formal landmarks. A new civic development at Community Park could bring this property prominently into the visible public realm. Any new development must maintain a direct street presence.



View of Battlefield Park with its strong landscaped image

In terms of style, architectural-style buildings should explore the house form vernacular with traditional or post-modern elements.



Church of The Redeemer on Lake Ave S., built 1877

Downtown South

(Lake Avenue, Mountain Avenue and Elm Avenue)

Like Jones Street, this area is a small scale, intimate urban environment closely linked with King Street and the core.

Heritage Features

Once south of King Street, one enters a quite distinctive area of special identity made up of a compact series of modest residential buildings intermixed with a series of medium scale residential and institutional buildings.

Built-Form

These streets are best described as a compact grouping of buildings, distinguished by their consistent massing and setbacks, and fine grain. The continuous, existing unscreened areas of parking in the area are regrettable. Residential parking arrangements do not impact unduly on streetscape amenity.

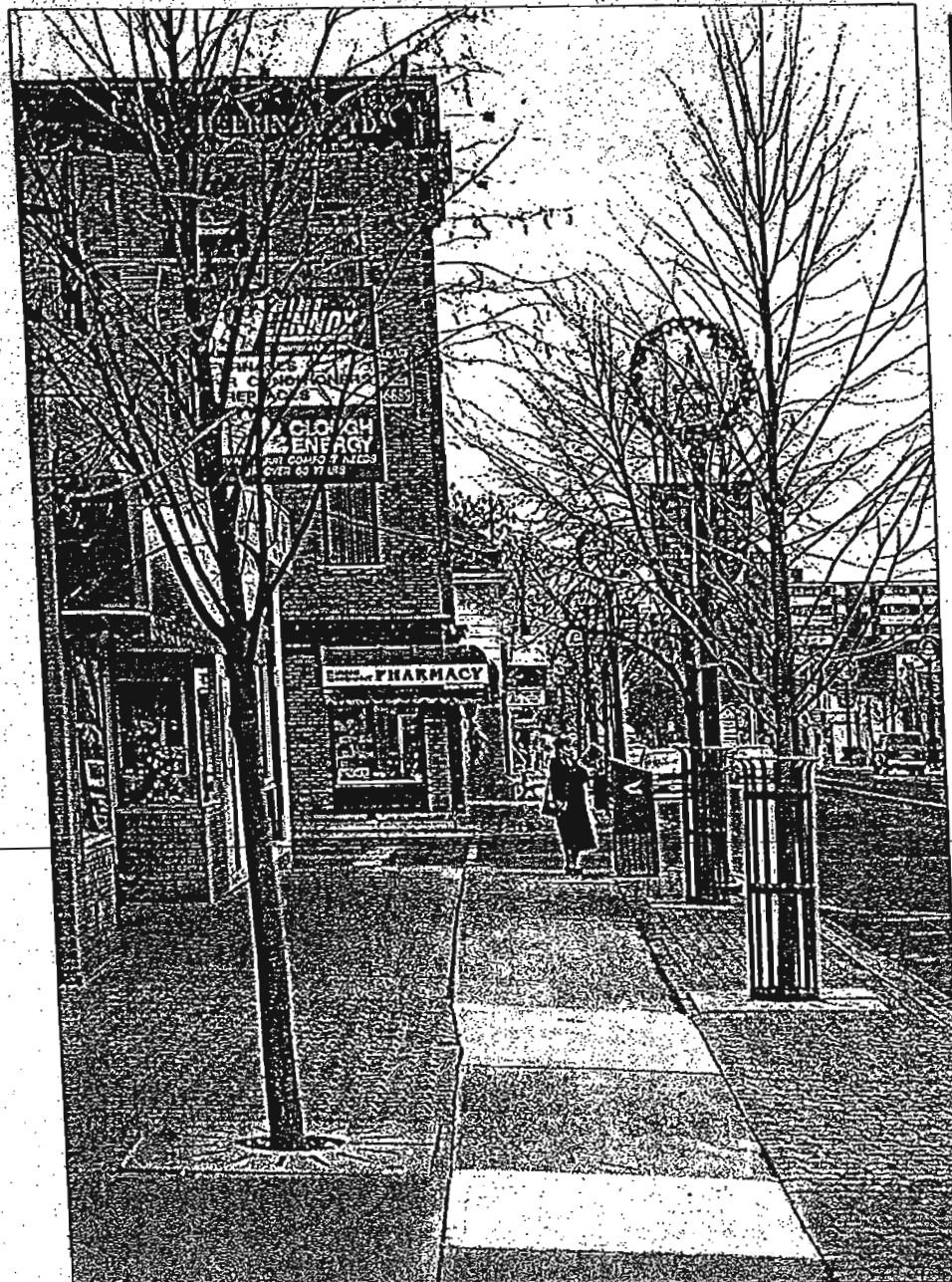
Design Theme

This area calls for a historically complementary theme similar to, but less elaborate than King Street. Any new development should have a strong street presence and strive for a high degree of lot coverage, not exceeding 4 storeys in height. A covered sidewalk in conjunction with new development along the west side of Mountain Ave. is desirable.

Downtown Core

Sidewalks are generally adequate in the downtown core. The improvements which have already been implemented between Lake Avenue and the Stoney Creek should be extended easterly to New Mountain Ave. and westerly to Second Street.

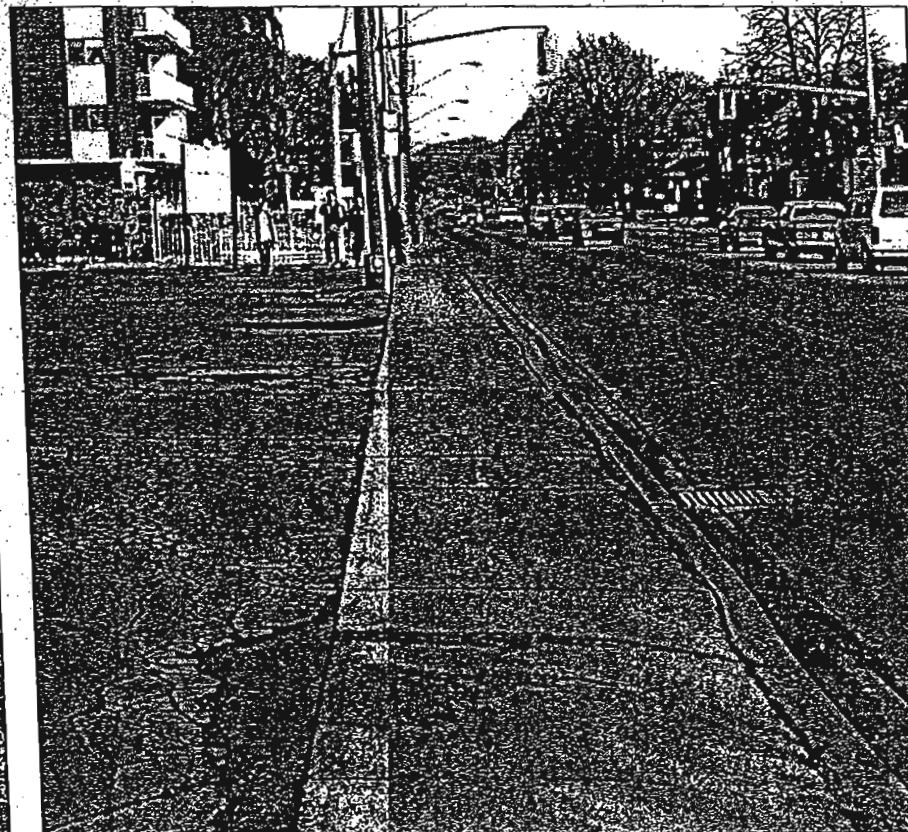
Sidewalk in Downtown Core



King Street West

Sidewalks within this area operate similarly to the core. It would seem that a relocation of the sidewalk to create a separation from the roadway is feasible in some locations, especially between Battlefield Park and the Battlefield Drive intersection.

Sidewalk on King St. West



Jones Street

The relationship of vehicular to pedestrian space works well on Jones Street. The opportunity exists to explore a street pattern which allows the roadway to be converted into a pedestrian area with limited vehicular access. Closure of through traffic at the Bridge would be necessary.

No walkway exists along the Creek and this matter must be addressed as part of redevelopment along its west bank.



Sidewalk on Jones St. at Bridge

Downtown South

Compact road widths and pedestrian sidewalks are provided, but are insufficient and below standard at certain locations. A covered arcade in conjunction with new development on the west side of Mountain Ave. would strengthen the urban form of this street.

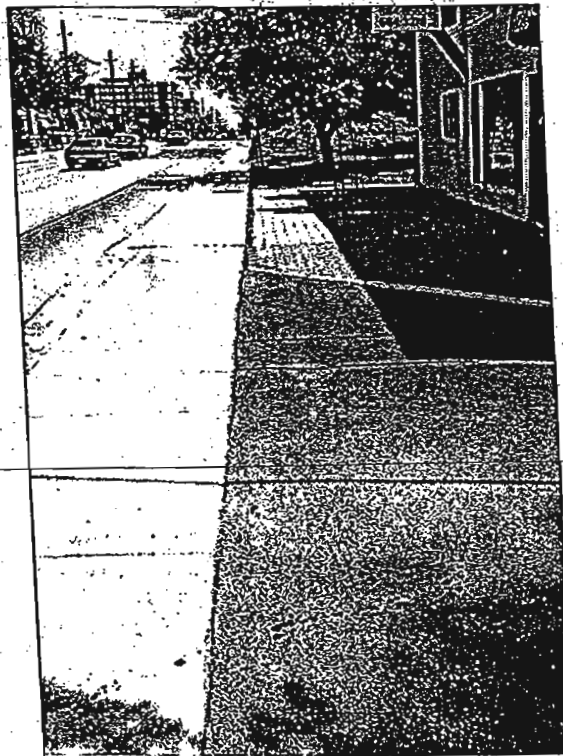


Sidewalk on Elm Ave.

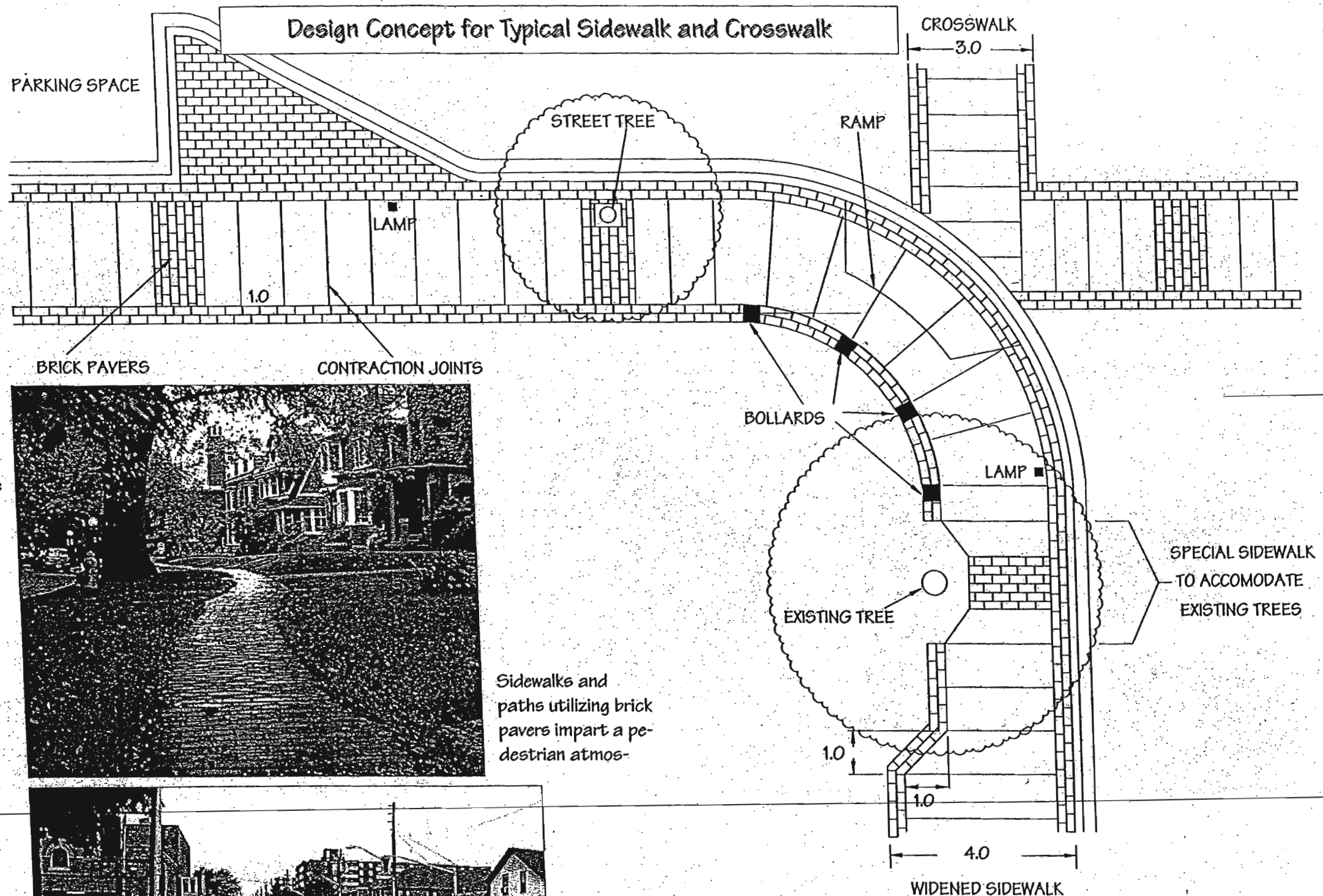
Paving

The design of paving provides suitable surface for pedestrians which is distinctive, durable and scaled for pedestrian use. In response to the design theme, interlock brick paving combined with scored concrete paving is recommended. The proportion of lock-brick to concrete will vary based on intensity of use. The design and scale of the paving surface will depend on the location, meaning that sidewalks will be widened or relocated in particular areas such as crosswalks.

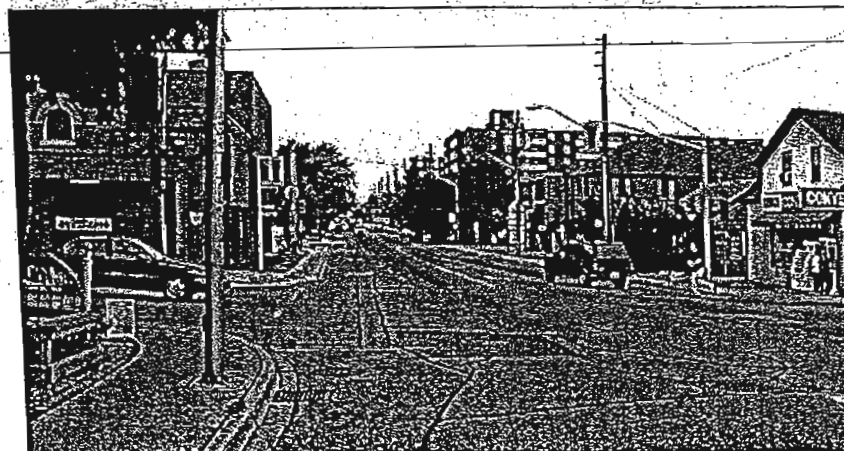
The paving pattern forms the strongest visual element of the streetscape improvements. The Plan suggests a 3 metre sidewalk bordered on both sides with brickwork, with crossbands every 10 metres and scored every metre. At the street corner, the brick expands to take over the crosswalk and identify the intersection. Crosswalks are identified as a pedestrian surface with concrete or brick instead of asphalt.



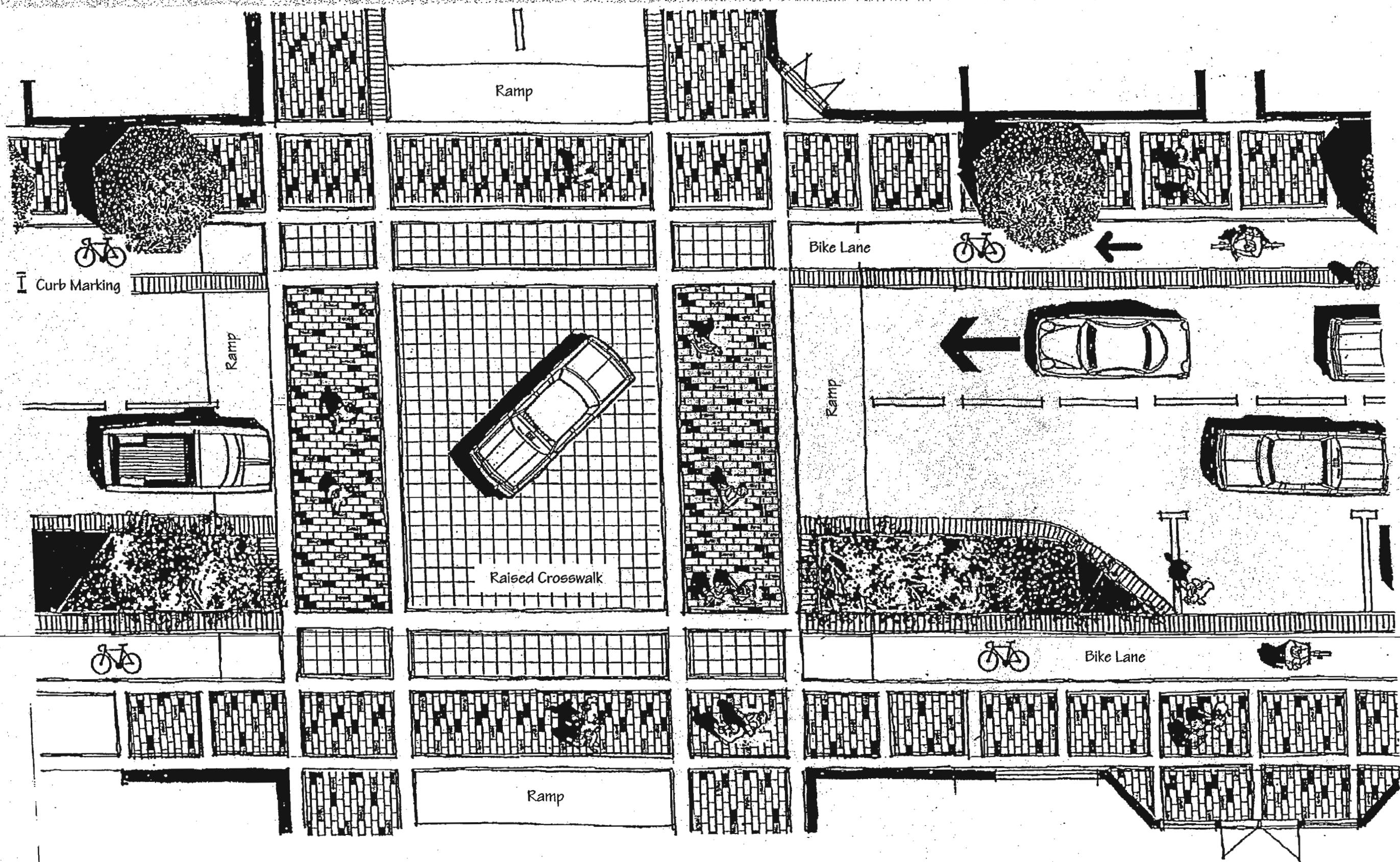
Sidewalk on King St. at Battlefield Park to be relocated to establish curbside boulevard.

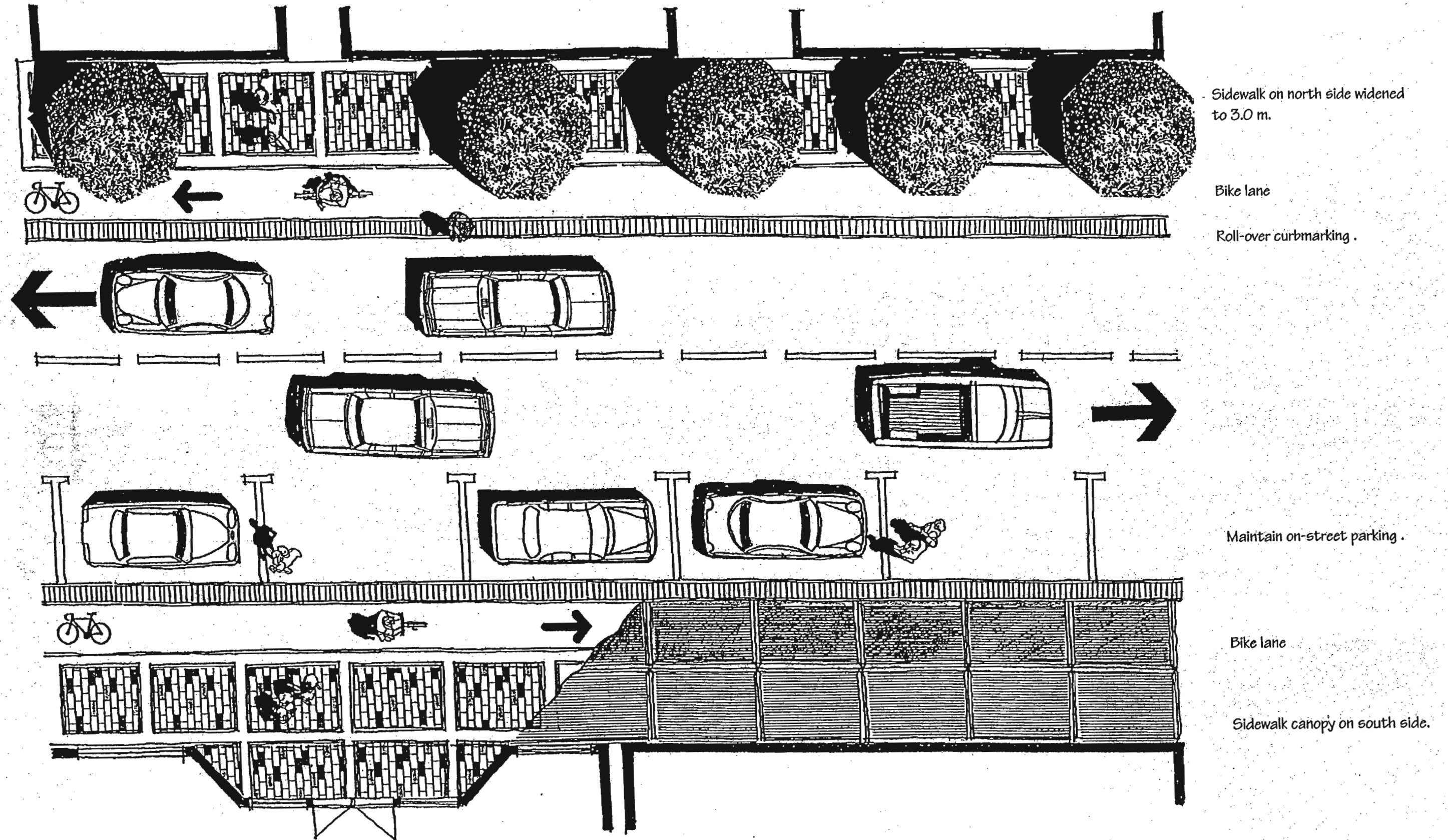


Sidewalks and paths utilizing brick pavers impart a pedestrian atmosphere.



Intersection at King St. at Lake Ave. to be improved with raised crosswalk.





Street Trees

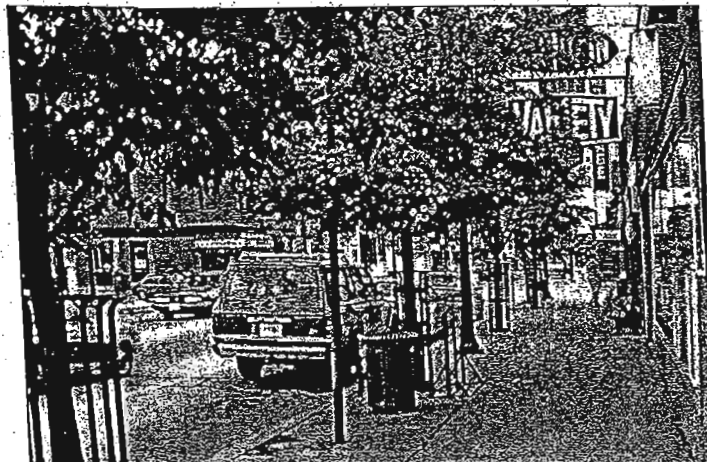
King Street has a strong landscape tradition, beginning with Battlefield Park and extending into the downtown with a number of mature street trees. This tradition should be further enhanced with new planting to fill the voids where mature vegetation has been removed. The relatively new planting in the core area is a good example of this. In addition to new planting, the existing vegetation and landscaping can be enhanced with more selective trimming. For example, a policy of less aggressive trimming of mature street trees by the Stoney Creek Hydro should be adopted. However, more formal trimming should be conducted along the Stoney Creek making it more visible.

The function of street landscaping and tree planting is to enhance the urban environment by tree form, foliage pattern, and colour and to provide spatial definition from the street of pedestrian routes and seating areas.

The primary types of planting required in the study area are large-crowned, long-trunked street trees set in the pavement, along sidewalks, in pedestrian areas, and in boulevards. Buffer planting and low, dense foliage to screen parking lots, for seasonal colour masses, and to define edges are also needed.

In public spaces and parks, displays, being a combination of shrubs, flowers, and trees, seasonal hanging baskets, and flower beds are needed.

Planting can contribute to the design theme by recreating the historical Stoney Creek orchard through the use of flowering street trees and by setting them on a regular rhythm along streets creating boulevards. Display planting areas should be located at gateways, and in public parks and parkettes.



Street trees in Downtown Core

Downtown Core

The existing street tree planting in the core is of an urban type standard and should be extended to New Mountain Road and to Second Street. As well, existing voids should be addressed to formalize the 10-metre planting module. Tree guards are recommended where trees are located in paved sidewalk areas. Currently, the use of tree guards is inconsistent in the area.

The vegetation overgrowth along the Stoney Creek at the King Street Bridge should be trimmed in order to enhance and formalize views of the creek and bridge.

Jones Street

In accordance with intention to convert Jones Street into a pedestrian area, the main thrust should be to retain the existing mature vegetation and to establish a formal vegetative pattern in conjunction with the redevelopment of the area. Judicious tree planting in a formal urban manner is suggested along in the centre of Jones Street as any redevelopment will likely take place at the street line.

Jones Street is crossed by Stoney Creek. The possibility of a linkage from the rear of 27 Jones St. to King St. along the Creek edge for pedestrians is highly desirable. Vegetation along the creek should not detract its visibility and should encourage a formal pathway.

The mature vegetation associated with the historical house at 1 Jones St. should be retained, however the site should be opened up to the street in order to establish a flower garden in the English tradition.

Downtown South

This area is predominantly a residential with a number mature trees at random locations with informal gardens. It is therefore appropriate to continue this pattern of landscaping. Formal street tree planting can be established along Lake Ave. and Mountain Ave. in conjunction with any new development. Interface with the Stoney Creek is possible on Elm Street where the proximity of the Creek to the street makes a public open space feasible, this should be explored in conjunction with the reconstruction of Elm Street. It is also important to screen the various parking areas from the pedestrian view this dense vegetation and flowers.

King Street West

Street tree planting in this area is dominated by individual large trees at irregular intervals with large gaps with no vegetation. The south side has greater vegetation as the existing structures are former dwellings which have generally maintained a grassed front yard. The north side however has large areas devoid of vegetation dominated by unscreened parking areas. A sidewalk boulevard with street trees should be established in these areas. In addition the aggressive trimming by Hydro should be curtailed.

Battlefield Park, Battlefield Creek Bridge, Smith's Knoll Memorial and Community Park provide interesting possibilities of creating formal landscaped areas. Existing mature vegetation should be retained where appropriate and enhanced with additional formal planting such as flowers and decorative trees.

Planters

Planters are a quick and easy way of getting planting into the downtown without getting involved in underground servicing, poor soil, road salt and the hazards of snow removal equipment, but they are not always satisfactory from an aesthetic and functional point of view. Their use should be restricted for genuinely temporary tree planting applications, or where they can be used successfully with low shrubs and flowers.

Planters should be personalized by casting the name and/or crest of the City and should be as small as possible so as to avoid obstructing pedestrian flow especially on sidewalks. Where planters are used for low shrubs and flowers, they should be clustered for the best effect either with other planters or with benches and receptacles. They can be used to define areas from the main pedestrian traffic flow, such as widen sidewalk areas, bridges, park entrances or the proposed Town Square.

There are a number planters in the study area, some of which are effective such as the planter at the Elm-King Plaza. Others are not as effective, such as at the Lake Ave./King St. intersection. In conjunction with the reconstruction of sidewalks, planters along King Street should be addressed in terms of location and design.

Facades

Downtown revitalization can only occur if both the public and private properties are improved. Facades are primarily within the private realm and, therefore, cannot be fixed to the same extent as public properties since it is the prerogative of the property owner to improve his property as he sees fit.

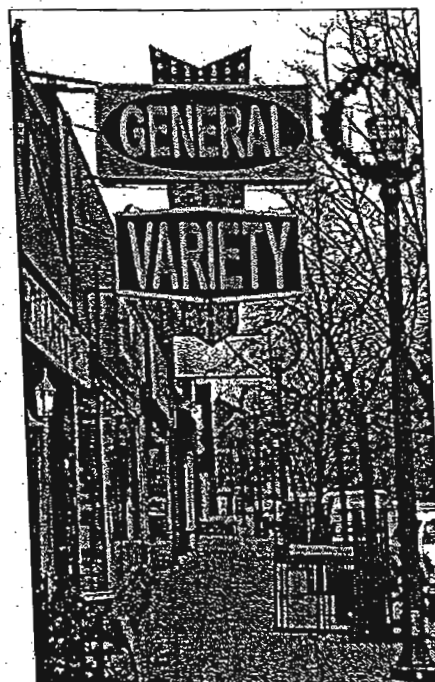
This section simply identifies the examples of improvements which will fit the design theme and respect other properties as well as create a visually cohesive and attractive area. It is important to understand that the facade of a building is a link in the overall chain which makes up the streetscape. The design should not make the building stick out too much to call attention to itself in the delusion to create a marketing advantage. It is useful to utilize what is there already, i.e., clean up bricks and stone instead of covering with siding.

Section 6, Elevations, of this Plan, provides design options which illustrate the principles which may be pursued by property owners should they choose to renovate.

Signage

Signage is often the main source of visual confusion in downtown areas and the Stoney Creek downtown is also vulnerable to inappropriate signage. The major objective should be to coordinate public and private signage so that the individual messages are easily perceived and identified and the effect is cohesive. It is recommended that the City's Sign By-law be revised in order to regulate signage more effectively within the study area and that public signs also follow a specific uniform standard.

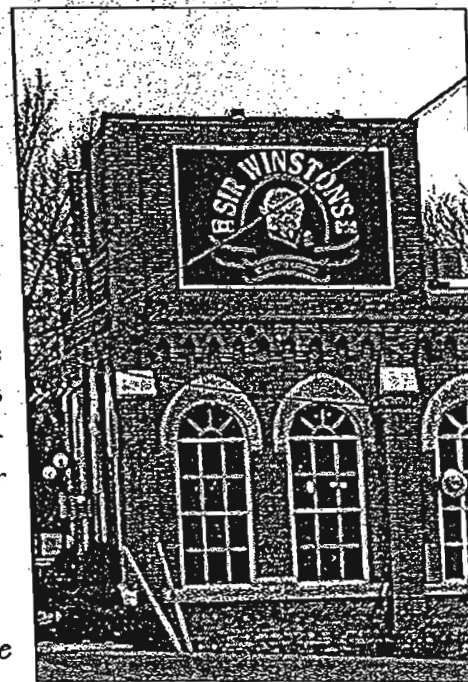
One problem with tightly controlled signage is that may regulate so rigidly that spontaneity, creativity, and variation is lost in the process. To avoid this pitfall, the signage guidelines occur in two parts: guidelines should relating for private advertising and building signage which allow for variation, and more controlled standards relating to public signs for buildings, parking lots, parks, downtown symbol signs, and street signs.



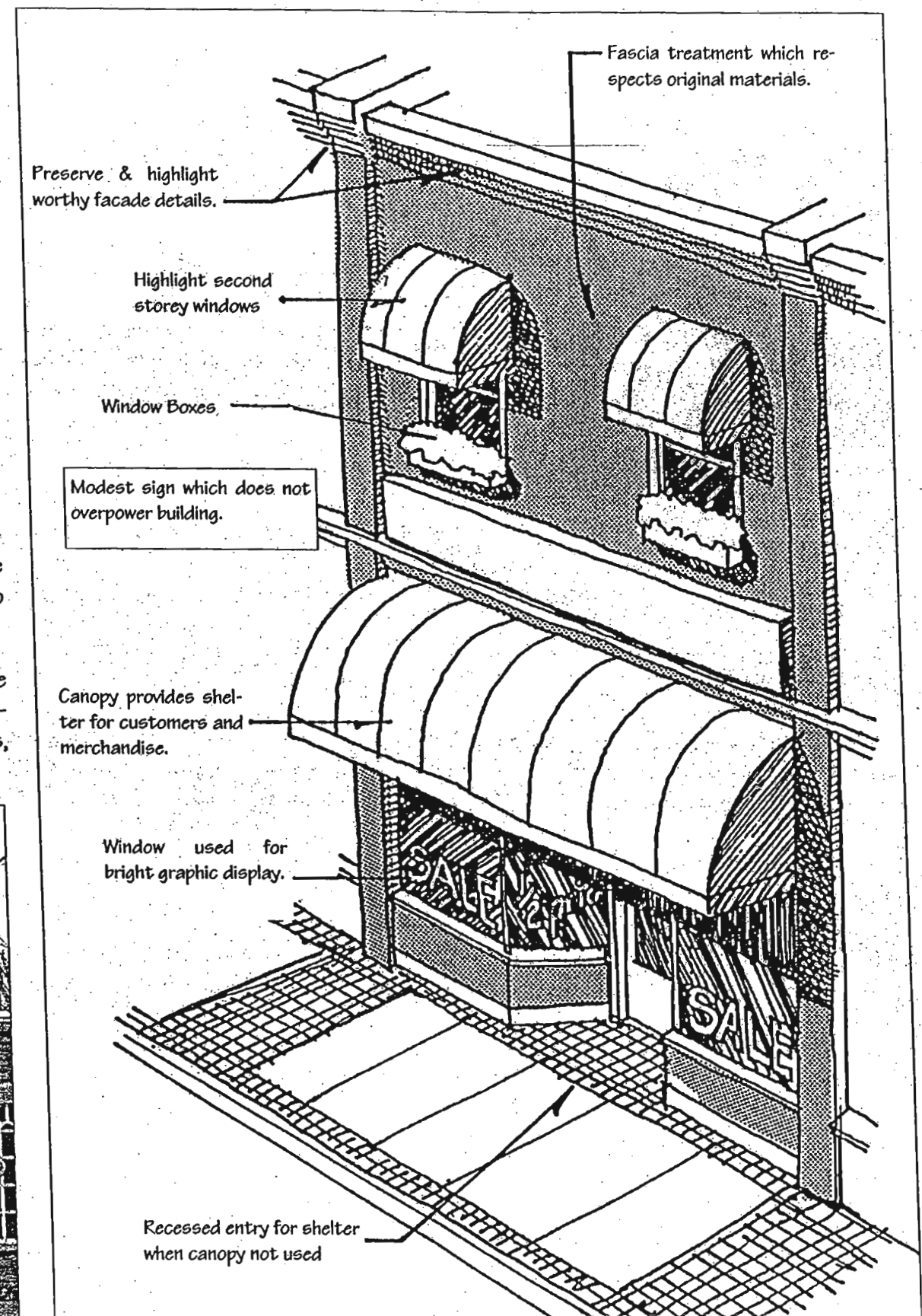
Private Commercial Signage

In considering the design theme, historic advertising signs have taken many forms, but generally the lettering is of a simple classic type, construction is of traditional materials, and the size is not too excessive. In addition to size and location, the Sign By-law should limit letter scripts to a number of classic fonts and colours to a certain palette as shown in the illustrations of this Plan. Business signs should be affixed to buildings and have regard to architectural features and effect on the streetscape and colour and design, and illumination should relate to the building.

Typical signage



Theme signage



Public Signage

The Stoney Creek city colours are blue and green on white. This colour combination is attractive and messages will be readily perceived if blue or green lettering is placed on a white background for smaller signs, and vice versa for larger signs. This colour combination is therefore recommended for all signs under the public domain. There are a number of different public signs in the study area which must be considered.



Street Signs (Completed Summer 1998)

Street signs in study area are contemporary, being green with white letters. A special downtown street sign which uses the style of the traditional street signs with black metal surround and finial mounted on circular black metal poles is more appropriate. The lettering should be blue on a pale background and incorporate the name "Stoney Creek".

Traffic Signs

Wherever possible traffic signs should be clustered on a single pole to minimize visual clutter created by numerous standards each with its own sign and where traffic signs require their own pole, a black finished metal standard matching the lamp and informational sign standards should be used. In other cities, this kind of colour coordination has been achieved by using a black vinyl sleeve which slips over the pole.

Identifying Signs

For signs identifying a place or entrance a double-faced, black finished metal standard system into which panels may be placed, added or replaced as required is recommended. The City colours of blue, green and white should be consistently used: white background, blue or green lettering.



Parking Lot Signs

Identification for the parking areas should be coordinated in colour and letter style with the street signs: white background with blue lettering or symbolic "P".

Downtown Symbol Signs

The existing downtown Stoney Creek promotional sign utilizing the monument represents a good design in itself and can be well co-ordinated with a signage system for the remaining area. As these signs come up for replacement, however, they should be redesigned to co-ordinate more completely with the graphics system illustrated for other signs.



Downtown Core

The downtown core currently has a wide variety of signage, both stylistically and locationally, which compromises the architectural cohesiveness of the area. To remedy this, a number of principles are suggested to be incorporated into the Sign By-Law as regulations for the area along King Street between Second Street and New Mountain Road. They are as follows:

- Commercial signs to be building mounted, without projecting into the public space.
- Public signage in accordance with specific, common design elements.
- Consolidation of public signage as much as possible.
- Establishment of colour palette and font styles for public and commercial signs.
- Encourage co-ordination among proprietors with respect to signage.

King Street West

King Street West also has a wide variety of public and commercial signage, but less intense than the core. As previously stated, King Street West is more contemporary, however signage in keeping with the historic design theme would be appropriate. In this area, signage is generally smaller scale and ground mounted. To co-ordinate this better, a number of new regulations are suggested for the Sign By-Law for the area along King Street West between Centennial Parkway and Second Street. They are as follows:

- Signs to be small scale and constructed of traditional materials with historic design elements.
- Signs to be ground mounted, without projecting into the public space.
- Consolidation of public and commercial signage.
- Establishment of colour palette and font styles for public and commercial signs.



On Street Parking

On street parking presently exists at various locations along King Street and on side streets. The focus of this analysis will primarily be for the King Street corridor.

On street curb parking, while it has existed for many years on King Street has only recently been formalized with pavement marking. On street parking functionally reduces King Street to a two lane roadway (one lane in each direction), from a four lane section for most of its distance in the study area, particularly in the core area. There are instances, at intersections where parking is not permitted in order to facilitate turning lanes. There are time limit restriction in place, however no parking meters currently exist.

On street parking is supported from an urban design perspective as it has traditionally existed in the area and imparts a distinct urban flavour to the streetscape. As well, as it functions, physiologically, as a separator of the pedestrian and vehicular traffic flow and slows traffic which is also an objective of the this plan.

Visually and functionally, the King Street parking scenario can be enhanced with curb and brick pavement delineation, such as shown in the image below. This would facilitate the introduction of additional landscaping and street furnishing elements as well as the widening of sidewalks at certain locations. It is recognized that the delineation of on street parking with curbing and paving is an expensive undertaking and should therefore be considered at the time of road reconstruction.

Downtown Core

Delineation of on street parking in the downtown core should be of an urban type standard primarily consisting of curbing and brick style pavers together tall branching street trees and street furnishing. In this area the curb delineation should function as an extension to the sidewalk area.

King Street West

Delineation of on street parking on King Street West should be "less" urban type in nature with greater emphasis on its landscape character in order to draw attention to its role as a promenade into the downtown. Here, the delineation of on street parking should consist of curbing, brick style pavers together with a mix of colorful street planting. In this area the curb delineation should function as an extension of the planted boulevard.

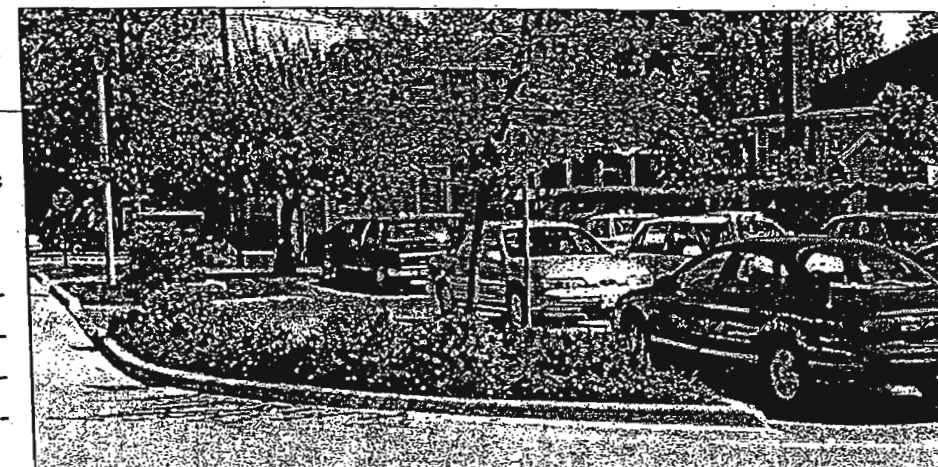
Parking Meters

On street parking meters are currently not used in Stoney Creek. If, and when, they are implemented they should be colour coordinated with the design theme. The support poles should be a uniform steel pole, with a durable black finish to match sign standard.

It is advisable to cluster two meters on one pole to reduce the overall number of units and care should be taken in their placement to avoid conflicts with other streetscape elements such as street trees and lamp standards. They should be set back from the curb to match the street alignment of trees and lamp standards.



Unscreened off street parking on King St.



Example of screened off street parking

Off Street Parking

One of the major contributors to a "suburban" downtown image is exposed surface parking lots. In order to retain convenient off street parking while at the same time maintaining a cohesive visual effect, parking lots must be heavily screened around the perimeter with mass landscaping, particularly when visible from King Street or when adjacent to a pedestrian space. Pedestrian surface treatment should start within the parking lot and extend across streets and adjacent developments. Corners that are not accessible to vehicles should be treated as parkettes and landscaped islands. Driveway access from King St. should be precluded.

Lighting



The objective of lighting in the study area is to create a distinctive quality in both light performance and fixture appearance so that it is recognizable from the rest of the City. A program should be developed for the phased replacement of pole mounted lighting fixtures over a period of years.

Streetlights should respond to the design theme with elements of the luminaries which have an historic derivation. The scale of the fixture should relate to pedestrians, and therefore be lower than standard street lights mounted on hydro poles.

There should be no visible overhead wires since early lamps were gas lamp style, the light quality should be from an indirect source to reflect the gas lamp quality.

The lighting level of each fixture should be sufficient so that they can be spaced reasonably far apart in the interest of cost effectiveness. At the same time, the optic system should be indirect to reduce glare and throw light to the ground so as to eliminate light penetration into second storey windows.

Spacing will vary according to the intensity of pedestrian use and the City's standards for various street types. For the typical situation, a staggered spacing of approximately 10 meters should achieve the required light level for King Street. The spacing of light standards sets up a basis for a rhythm of all the street furnishings, and a module of lamps, paving strips, and street trees can be developed.

Streetlights should be close to the curb and corners to allow free pedestrian movement and to act as poles for banners projecting at right angles to the street.

The existing product used in the BIA area achieves the above noted criteria and would generally be a suitable standard to adopt for the remainder of the study area. This streetlight standard may be altered in special areas such as bridges and pedestrian squares where they are intended to denote a distinct space or architectural element.



Utility Wires

Generally there has been a movement in most communities to eradicate downtown areas from the clutter and visual confusion of overhead utility wires and numerous pole installations. The appearance of much of the study area is compromised by the presence of wires, poles and transformers, particularly in the proximity of historic sites and open spaces. In addition the wiring has a negative effect on mature vegetation which must be aggressively trimmed in order to not interfere with the utility.

It can also be observed that much of the existing pole installation is inappropriately placed from a pedestrian point of view.

A program should be developed in conjunction with the hydro utility to bury and/or consolidate wires and transformers over the long term in conjunction with road reconstruction.

Downtown Core

Street lighting in the BIA section of the downtown core has already been converted to suitable luminaries. This standard should be extended easterly to New Mountain Road and westerly to Second Street. The utility wiring and transformers should also be removed from this area in the short term so that the downtown is better perceived as a high quality, cohesive unit.

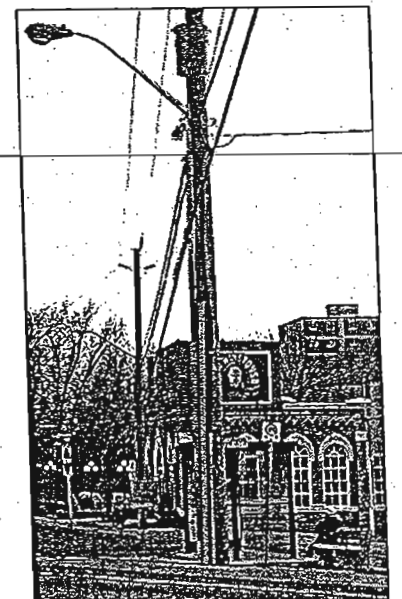
King Street West

King Street West has the standard suburban luminaries mounted on hydro poles. There is also an over abundance of overhead wires and transformers. A program to replace these with a more decorative luminary is suggested in conjunction with a long-term program to bury the hydro lines or at least coordinate and reduce their numbers.

Special areas such as the Smith's Knoll, Battlefield Park and the Battlefield Creek Bridge should have unique lighting representative of their strong historic significance.

Jones Street

Lighting on Jones Street should be converted to a finer scale luminaries similar to the downtown, but whose primary function is illumination of a pedestrian area. The standard chosen for this area can also be utilized for special areas such as the bridges, squares and open spaces. This may be undertaken in the eventuality that Jones Street is redeveloped and pedestrianized.



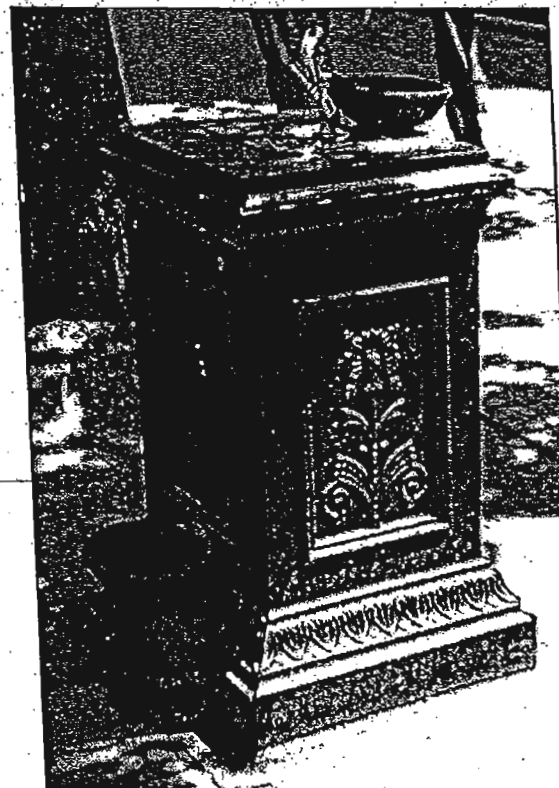
Seating

The function of seating is to rest on, but also to encourage social interaction by the way seats are grouped and located. While seating is primarily accomplished by providing benches, walls and planters can also be designed, with seating as a secondary function.

Benches of historic design are generally of cast iron supports with wooden slat seating surface and backrests. It is common to identify the name of the City in the wood. While there is seating in the study area it is of a wide variety with no common standard. The benches in the BIA area are considered appropriate for use throughout the study area.

Benches should be grouped together wherever possible to create "seating areas" and combined with planters, receptacles and lighting in order to complement and define the area. Seating areas should be located and arranged in a variety of sunny, shady, private, busy and quiet environments so that the user can choose according to mood.

Benches should be provided in all public spaces, in all widened sidewalk areas, at all bus stops where shelters are not provided, in the proposed Town Square, at Smith's Knoll, in open spaces and in places where space permits such as widened sidewalks.



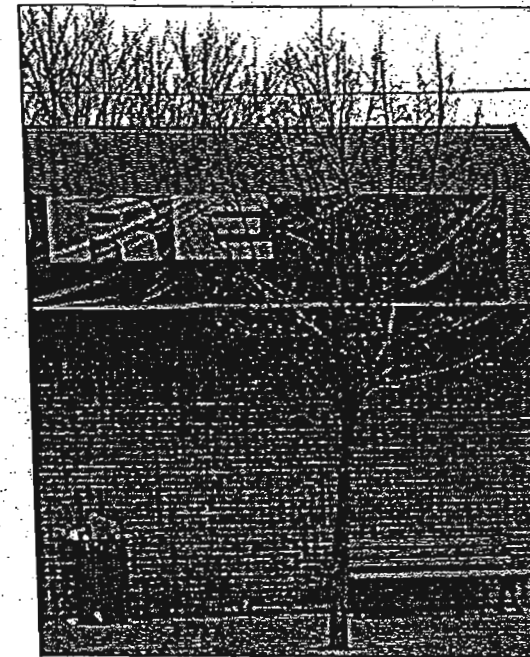
Drinking Fountains

While the supply of drinking fountains has a long and common tradition in Ontario, downtown Stoney Creek regrettably has none. Fountains should be placed in the proposed Town Square and in public parks and public spaces. At least one fountain should be of an elaborate traditional design to draw attention to the area while the others may be more conventional order to save expense.

Bus Shelters

The existing bus shelters, where they occur, are not appropriate to the downtown design theme. New shelters should be more appropriately designed and should incorporate large areas of plate glass in metal frames which relate in style and colour and lettering to area.

Shelters should be located at all bus stops where space permits without impeding the sidewalk area. The shelter can be tucked beside the main sidewalk surface.



Waste Receptacles

Visible garbage detracts from the aesthetics of the urban landscape. Well-designed and located waste receptacles are essential to the control of litter, and they also save time and money in maintenance.

In considering the design theme, historic precedents of waste paper baskets were generally open mesh or wrought iron open work such as those which exist in the downtown. These have the disadvantage of exposing the litter they are meant to contain. The main design theme guideline is to coordinate waste receptacles with other streetscape elements, particularly the planters.

Receptacles should be located close to high use areas such as pedestrian thoroughfares, rest areas, convenience stores and restaurants, seating areas and bus stops. In these high use areas, they should be plentiful, visible and convenient. They should be situated so that they do not interfere with pedestrian movement.

Utility Boxes

As Downtown electrical services are placed underground, a series of ancillary switching stations are required. Ideally, these should be underground, but this involves considerable expense.

Surface utility boxes should be screened from public view and yet accessible, at least on one side, for maintenance and servicing.

Bollards

The primary purpose for bollards is to prevent vehicles entering paved areas designed for pedestrians only, without posing an impediment to pedestrian flow. A secondary purpose is to act as a spatial definition between one area and another.

Bollards should be visible but not so tall as to dominate the scene and spacing between bollards should prevent access to small motor vehicles but easily accommodate other wheeled items such as wheelchairs and strollers.

In areas where service vehicles must have occasional access, bollards should be designed to allow removal and replacement.

The design of bollards should vary in various downtown situations so that they can reflect the mood of the area. In the core area, simple black metal posts are most appropriate as they relate well to the other street furnishing, such as sign posts, lamps, and containers. In park settings, a less formal timber bollard is more appropriate.

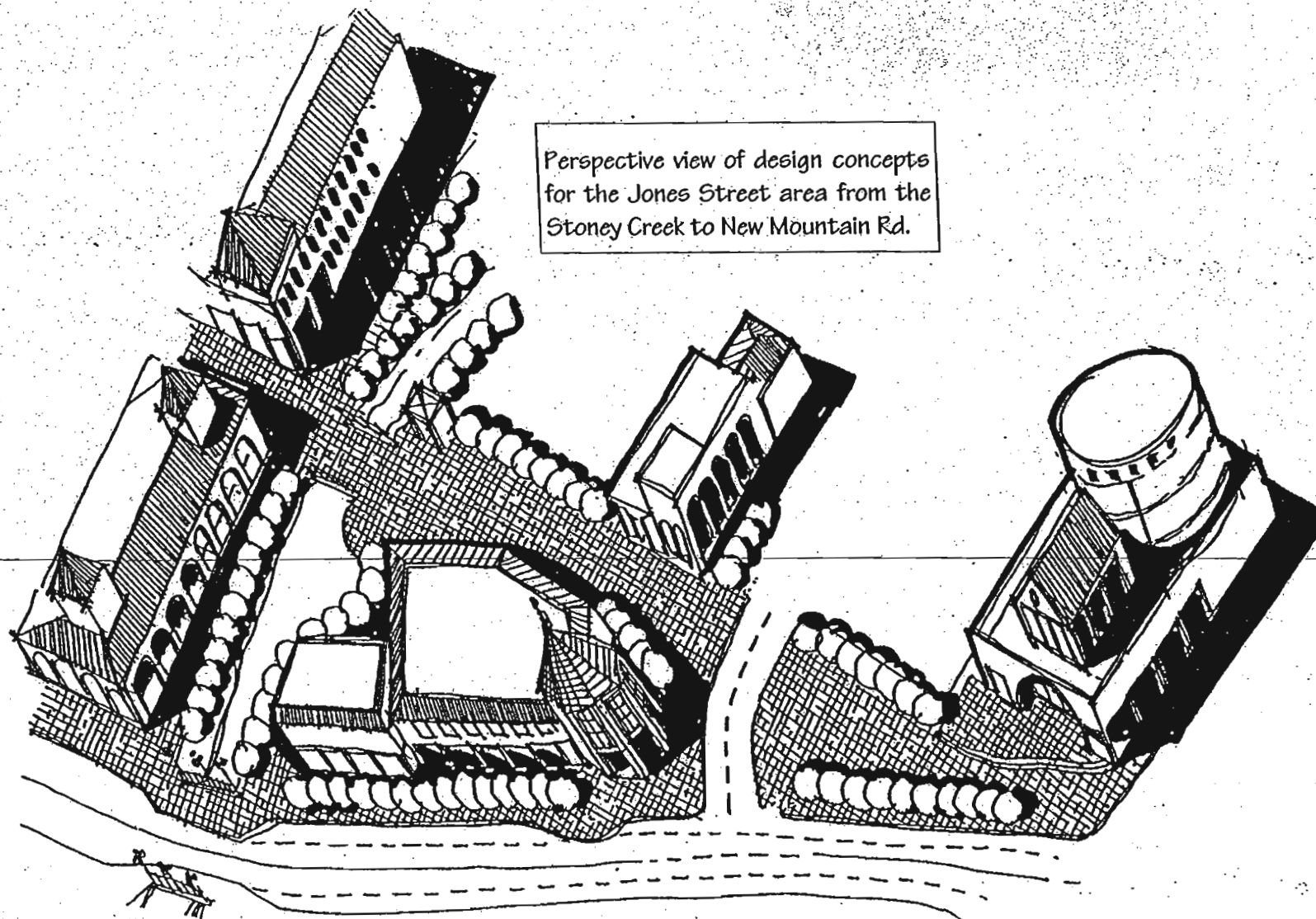
Fountain, Niagara-On-The-Lake

Section 3.0 of this Plan, entitled Urban Design Vision, contains a discussion relative to the importance of public spaces, design principles and a vision for the Stoney Creek downtown corridor via a sequence of public spaces. This section focuses more directly on those public spaces and landmarks.

Beautification

Beautification is a popular word that usefully describes actions aimed at making streets and landmarks more visually attractive and functional. It is also an important activity made necessary by the way we build and live. In suburban areas such as Stoney Creek, where landscaping and signage are often the dominant visual elements and the buildings tend to be tucked behind a parking lot, beautification is more important. As a general rule, the more urban the context the more important architecture is and the more suburban the setting, the more important beautification becomes.

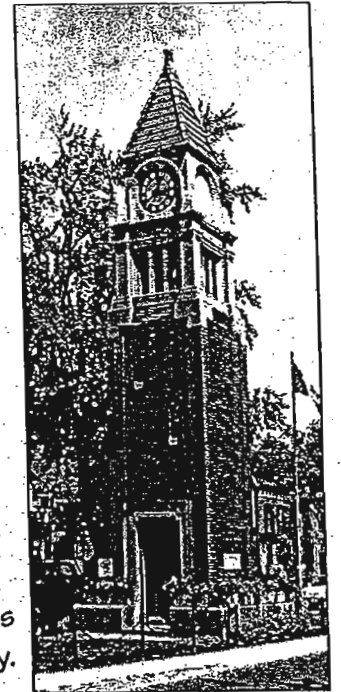
Whenever beautification efforts are undertaken, the nature of the efforts should harmonize with the setting. The many individual actions should be coordinated to enhance the uniqueness of the space, the area and the identity of the community. A strong sense of community is more likely to be formed where distinct physical images and landmarks are present. The design treatment of streets and public spaces can help provide the needed framework where these indicators are to occur. It is desirable for all citizens to have public spaces and landmarks that are visible and identifiable.



Perspective view of design concepts for the Jones Street area from the Stoney Creek to New Mountain Rd.

Public Spaces & Landmarks

Architectural landmarks impart distinct identity.



The public spaces and landmarks addressed by this Plan are long term in nature. Those under direct City control are tied to the 15 year implementation schedule. Others are contingent on private initiatives taking root. As such these may occur in advance of the City initiatives or much later. In any event, this plan establishes the design concepts and guidelines to be addressed when any development, public or private is to occur. The public spaces and landmarks, exclusive of the streetscape elements already discussed, are the following:

- Town Square
- Jones Street Bridge
- Jones Street Pedestrian Mall
- Stoney Creek Bridge
- Olde Town Centre & Market Area
- King Street Boulevard
- Community Park Complex
- Smith's Knoll Memorial
- Battlefield Park & Bridge

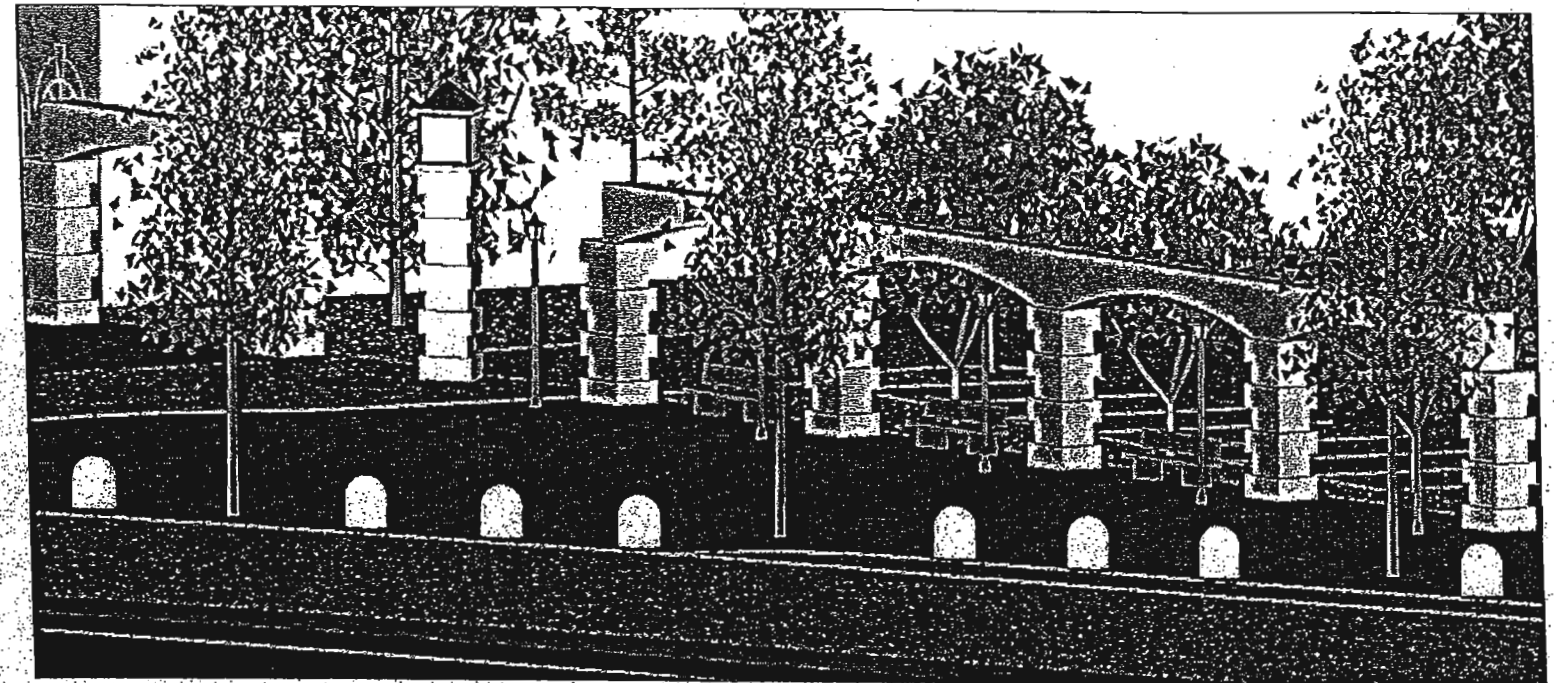
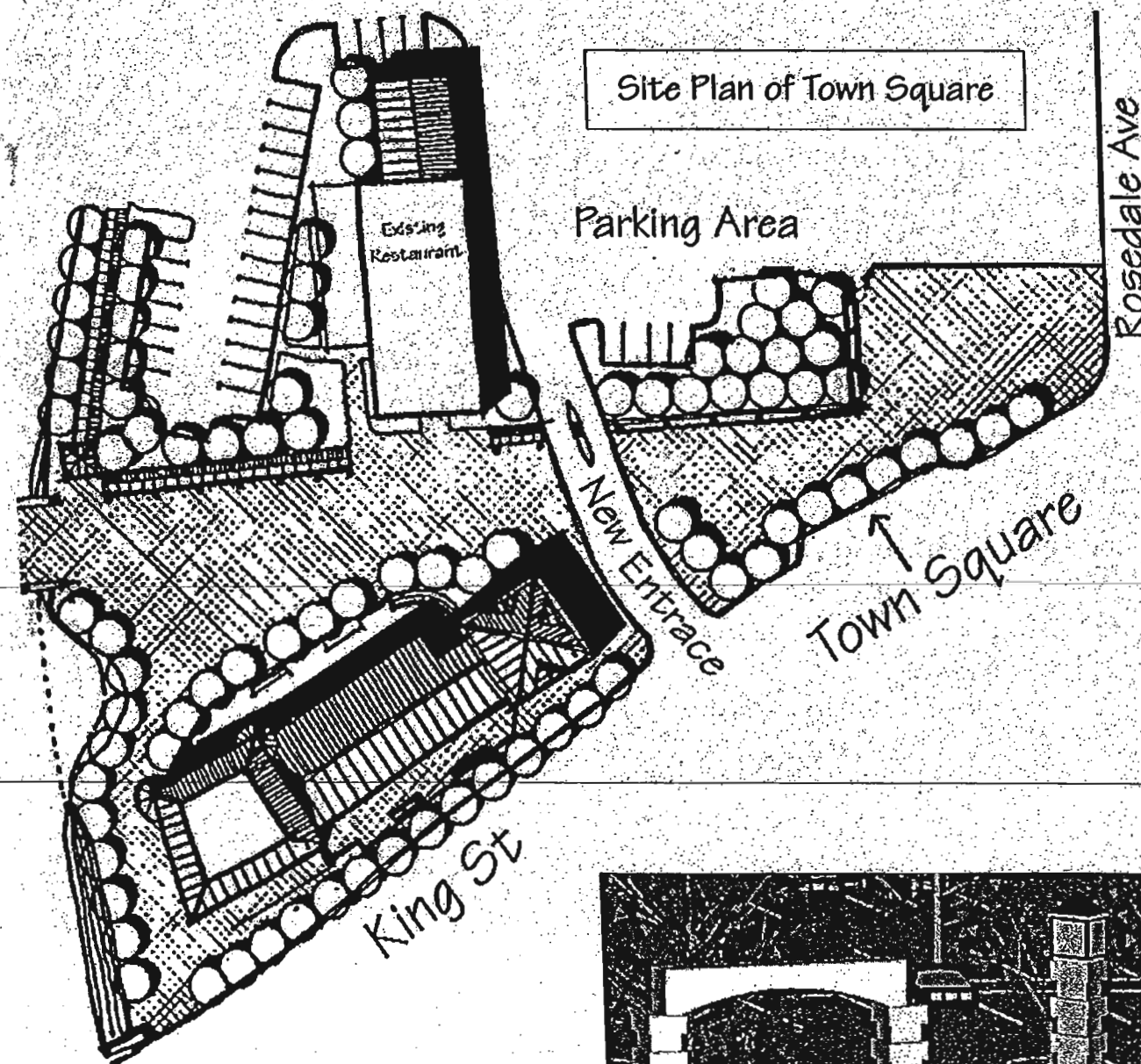
Many of the spaces identified by this Plan already exist, albeit they are not expressed at their full potential or former glory. The intent of this Plan is to provide the design parameters to allow the enhancement these spaces into high quality public spaces which serve as an amenity for citizens and travelers into the downtown and help define the identity of the community. Each area is unique in its own way; however, certain common elements must be incorporated into the design of each in order to tie together the corridor into one harmonious entity. This allows the individual to experience both something new and satisfy the need to see something familiar.

5.1 TOWN SQUARE

There is a need for a formal pedestrian-oriented public meeting place in the City to act as a focal point for the downtown. The meeting place conveys to residents and travelers something about the community. It must be distinct and of the highest possible quality.

Redesign of the Jones Street intersection at King Street, incorporating the delta property at 79 King Street, would allow for the creation of this Town Square.

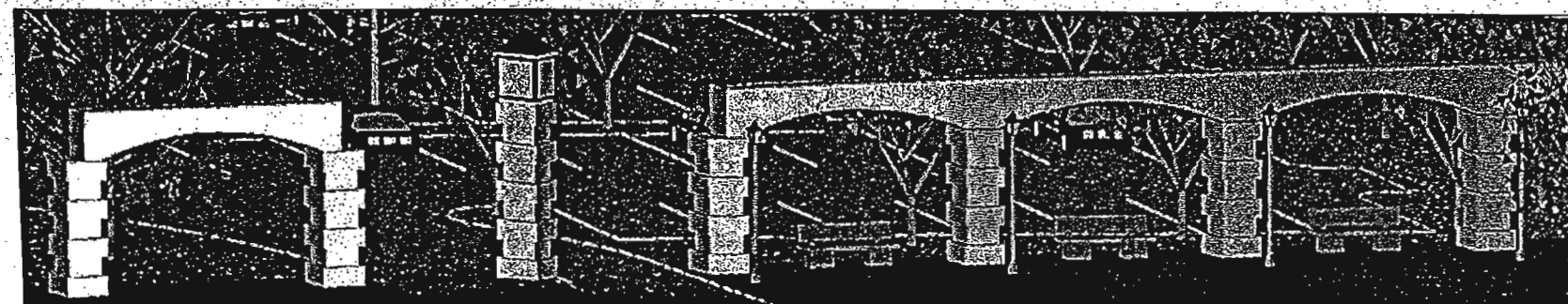
This square is at a strategic location to denote the entrance into the downtown from the west. It is highly visible from King Street and allows for views to be created westerly along Jones Street to the Stoney Creek.



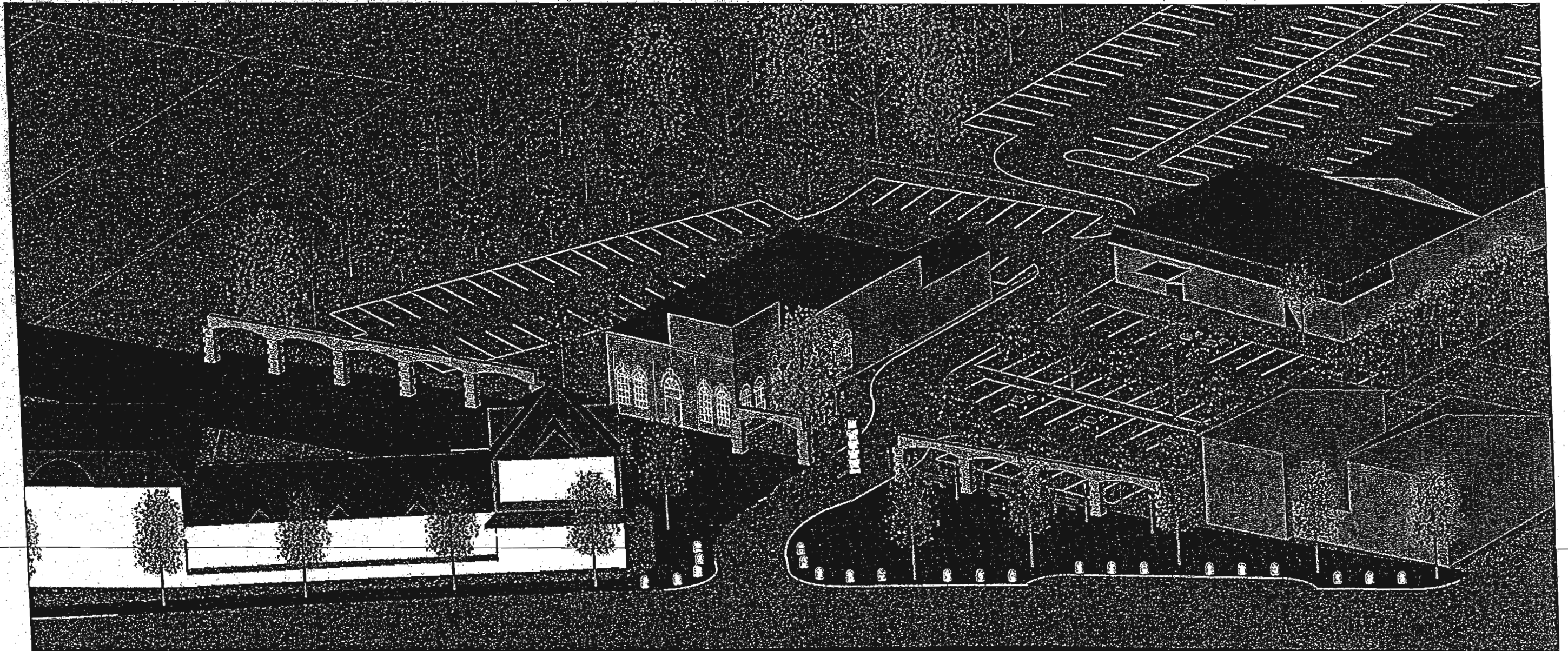
This rendering displays some of the elements to be addressed in the final design of the Town Square.

While the location of the Town Square is very desirable from an urban design perspective, there are a number of technical matters which must be addressed. The design elements integral to the success of the Town Square as a high quality focal point are as follows:

- Burying of all overhead utility wires and transformers and the elimination of all unnecessary poles.
- High quality curbing, bollard and paving treatment utilizing primarily lock-stone pavers with concrete borders.
- Enclosure with an arcade system of either brick or precast material. The surface grade is to be either sloped or stepped in order create a separation from King Street.
- Installation of pedestrian amenities such as benches, lighting, kiosk, fountain and a high quality bus shelter.
- Formal perimeter landscaping utilizing larger trees and dense mass shrubs to the north and flowering or ornamental type trees along the street line.
- The driveway into 27 Jones Street should not be separated by a curb, but with surface treatment only. The entrance must be denoted with an architectural feature with a relationship to the arcade.
- The facades of the existing building facing the square on the south should be enhanced given their new context.

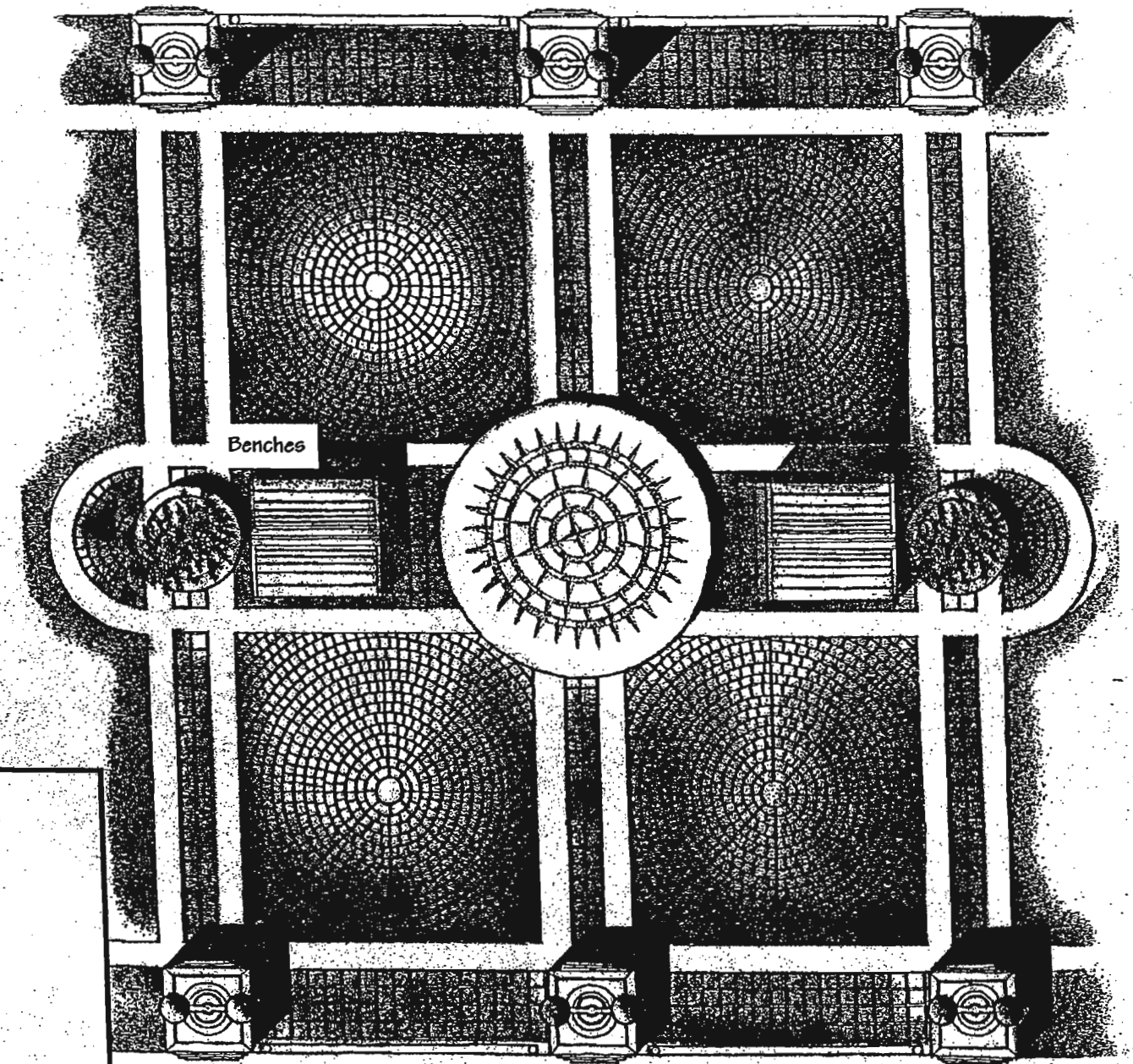
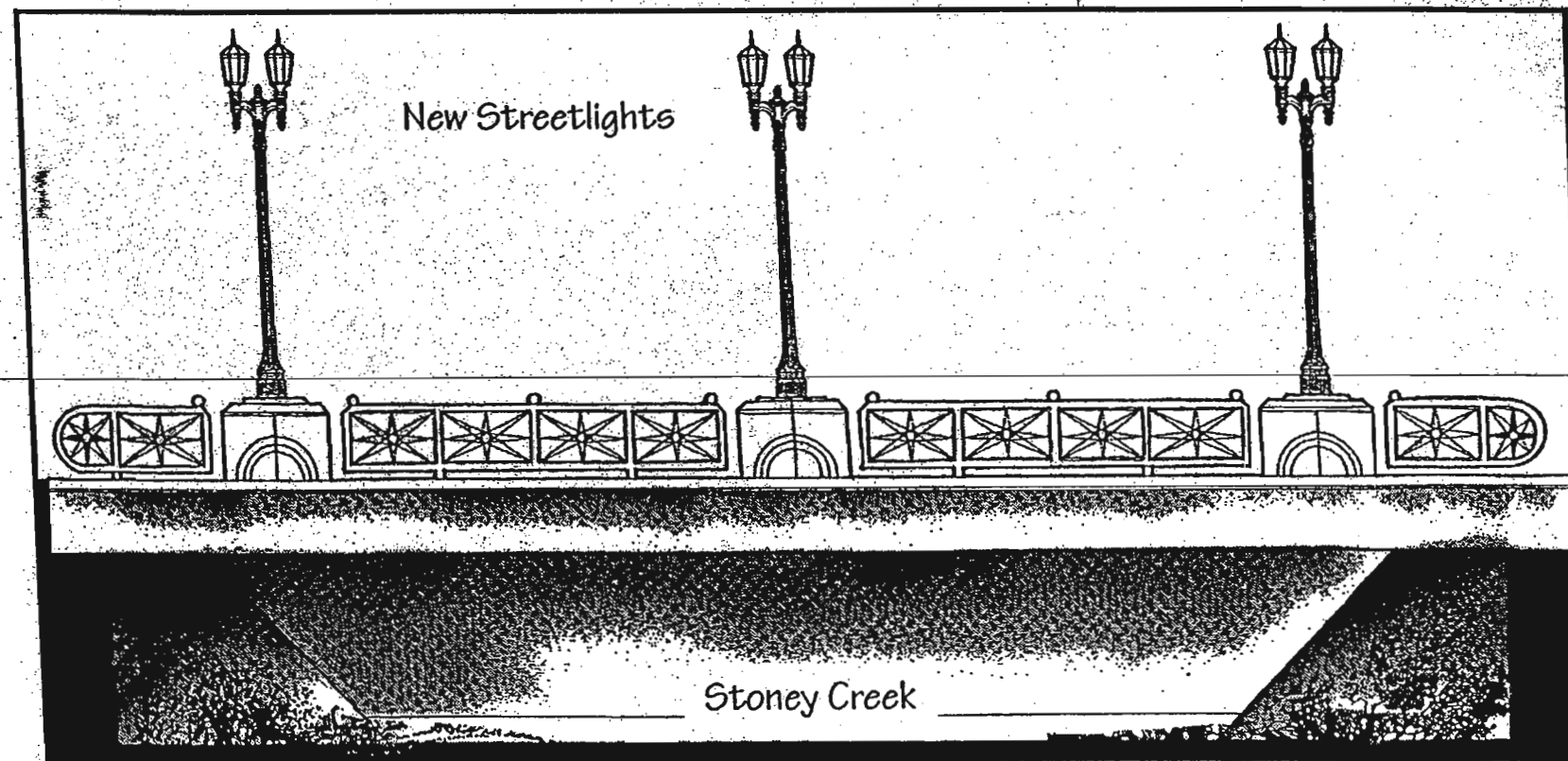
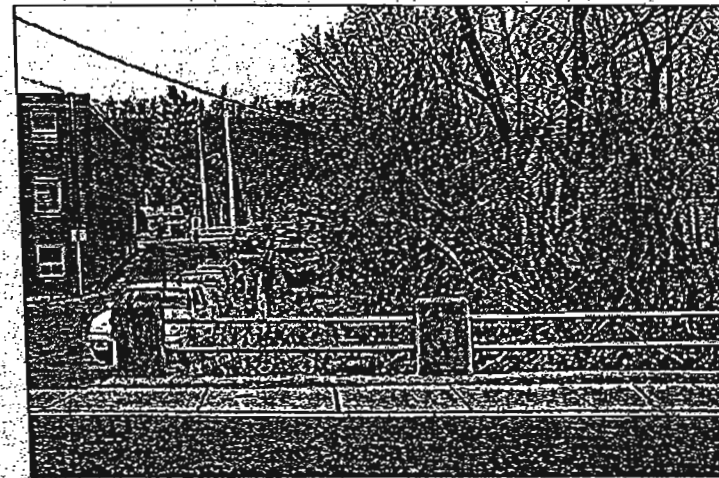


This rendering displays some of the arcade and entrance details to create enclosure and a sense of place.



This rendering illustrates the redesign of the Jones Street - King Street intersection, incorporating the delta property at 79 King Street. This allows for the creation of a Town Square at the delta. Also illustrated is a new development concept for the properties at 71 - 77 King Street East.

Closure of Jones Street to through traffic at the Bridge creates the opportunity for a pedestrian mall from the Stoney Creek to the Town Square. The Bridge will be embellished with a brick paver surface, railing and lighting treatment. The Jones Street right-of-way will also be similarly improved. The area will be a high quality public space and contain amenities such as public art and special streetscape and landscaping features to create a sense of enclosure.

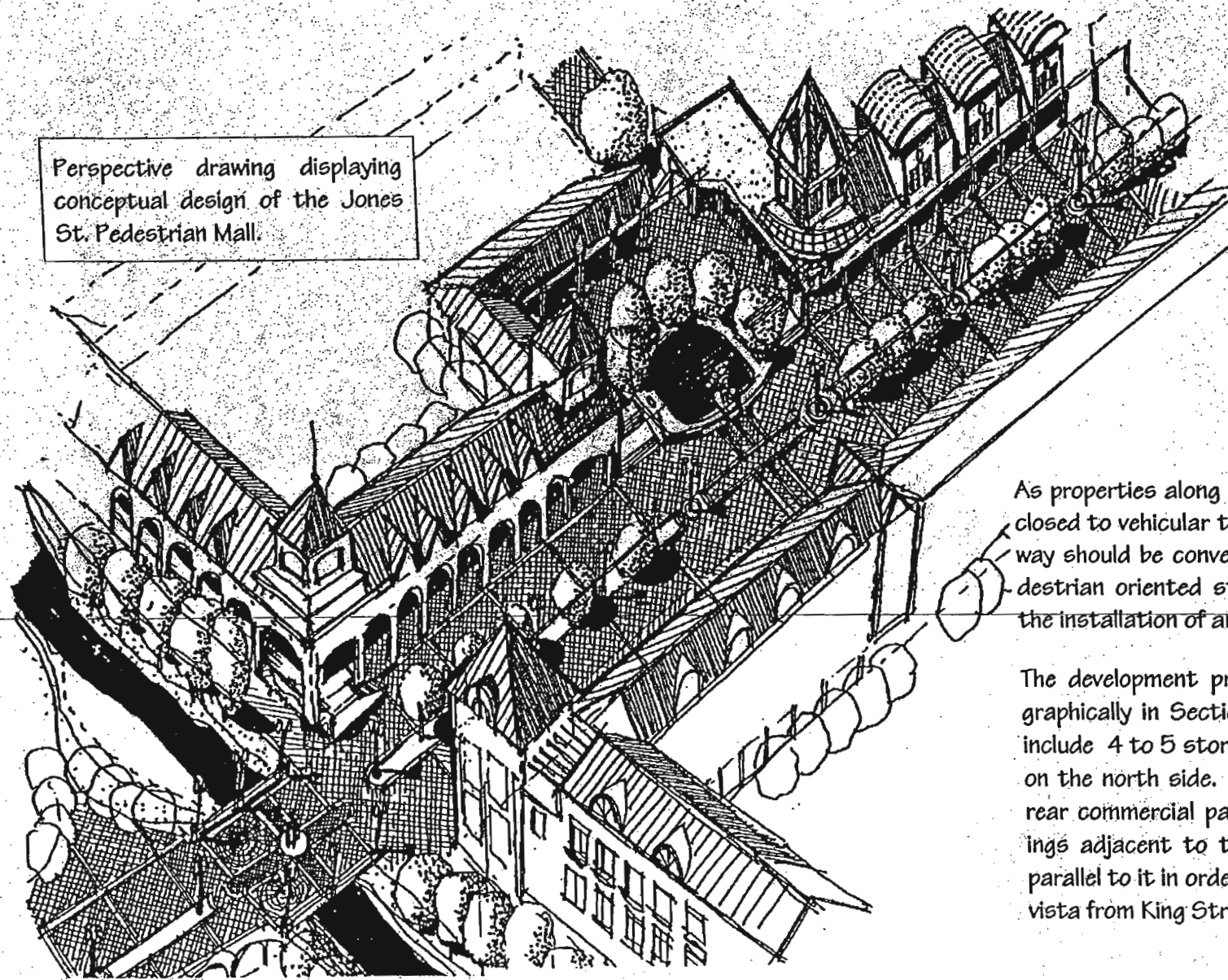


Pedestrian Bridge

As previously noted, closure of Jones Street to through traffic at the Bridge creates the opportunity for a pedestrian mall from the Stoney Creek to the Town Square. The Bridge is to be embellished and the creek enhanced and cleaned out to improve its visibility and aesthetic qualities. It is suggested that a weir structure be incorporated into the creek so that a regulated water flow is maintained.

The Jones Street right of way would be similarly improved to compliment and unify the bridge to the Town Square. The area will be a high quality public space and contain amenities such as public art and special streetscape and landscaping features to create a sense of enclosure.

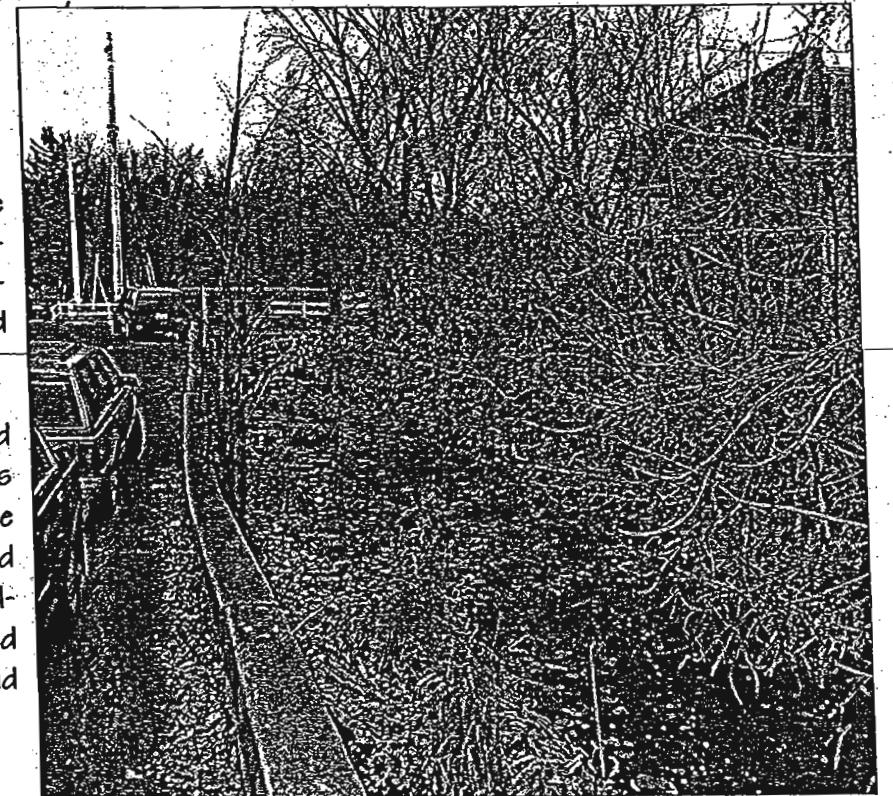
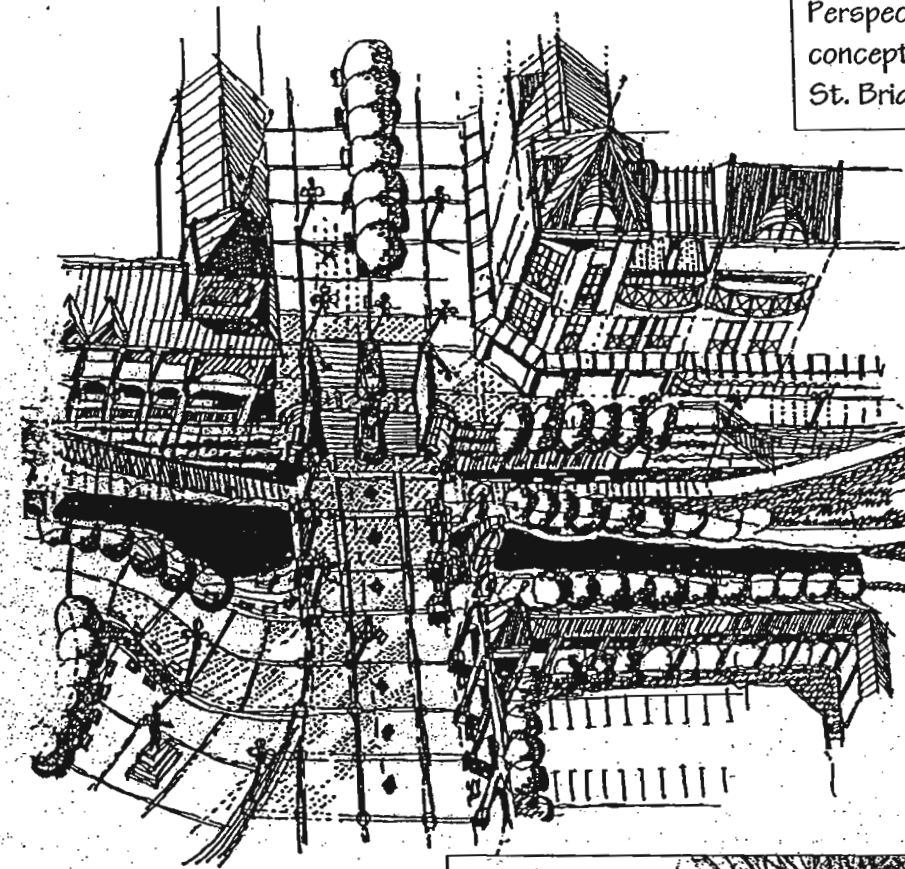
Perspective drawing displaying conceptual design of the Jones St. Pedestrian Mall.



As properties along Jones Street redevelop, it should be closed to vehicular traffic and pedestrianized. The roadway should be converted by resurfacing with pavers, pedestrian oriented street lights, centerline planting and the installation of amenity features such as seating.

The development principles for the mall are expressed graphically in Section 6.0, Elevations. These principles include 4 to 5 storey mix-use buildings at the streetline on the north side. Underground residential parking and rear commercial parking are essential. Also, new buildings adjacent to the Stoney Creek should be oriented parallel to it in order to establish a continuous linkage and vista from King Street to north of Jones Street.

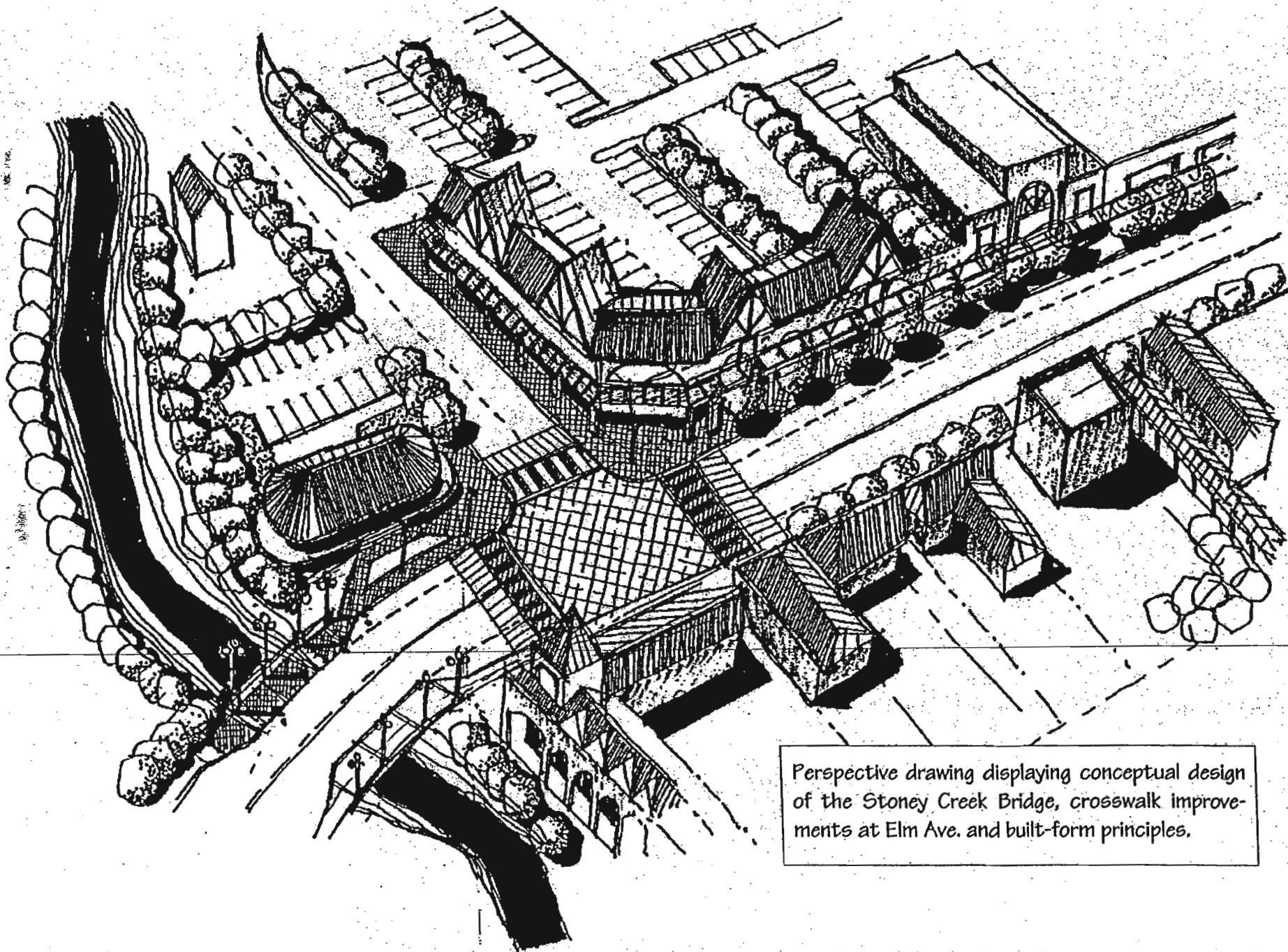
Perspective drawing displaying conceptual design of the Jones St. Bridge & Pedestrian Mall.



5.4 STONEY CREEK BRIDGE

Traditionally, bridges and watercourses represent a significant opportunity for the establishment of a visual sense of place. Stoney Creek Bridge at King Street is to be embellished to denote the main crossing of the Stoney Creek and to enhance views to the Creek. It is suggested that the sidewalk be widened at the bridge and that pedestrian features be incorporated. Vegetation on both sides should be trimmed to improve views.

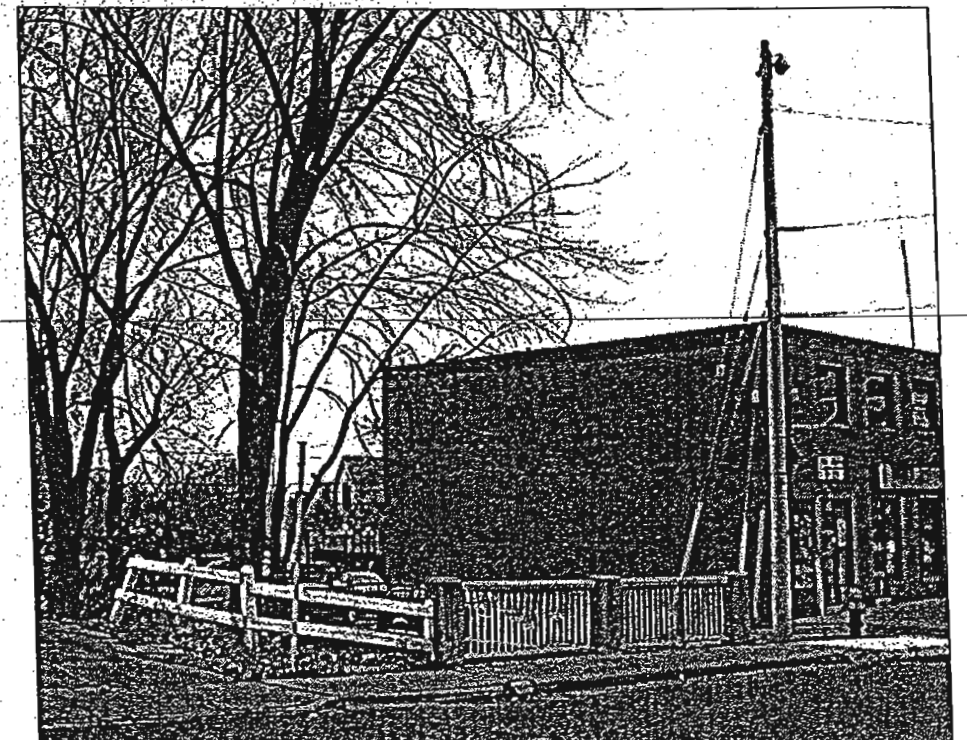
Extension of the "BIA" streetlight, curbing, landscaping and sidewalk improvements to New Mountain Avenue are also proposed to unify the core area streetscape. Ultimately, the driveway immediately east of the Bridge should be redesigned to allow the establishment of a viewing area along the east bank of the creek.



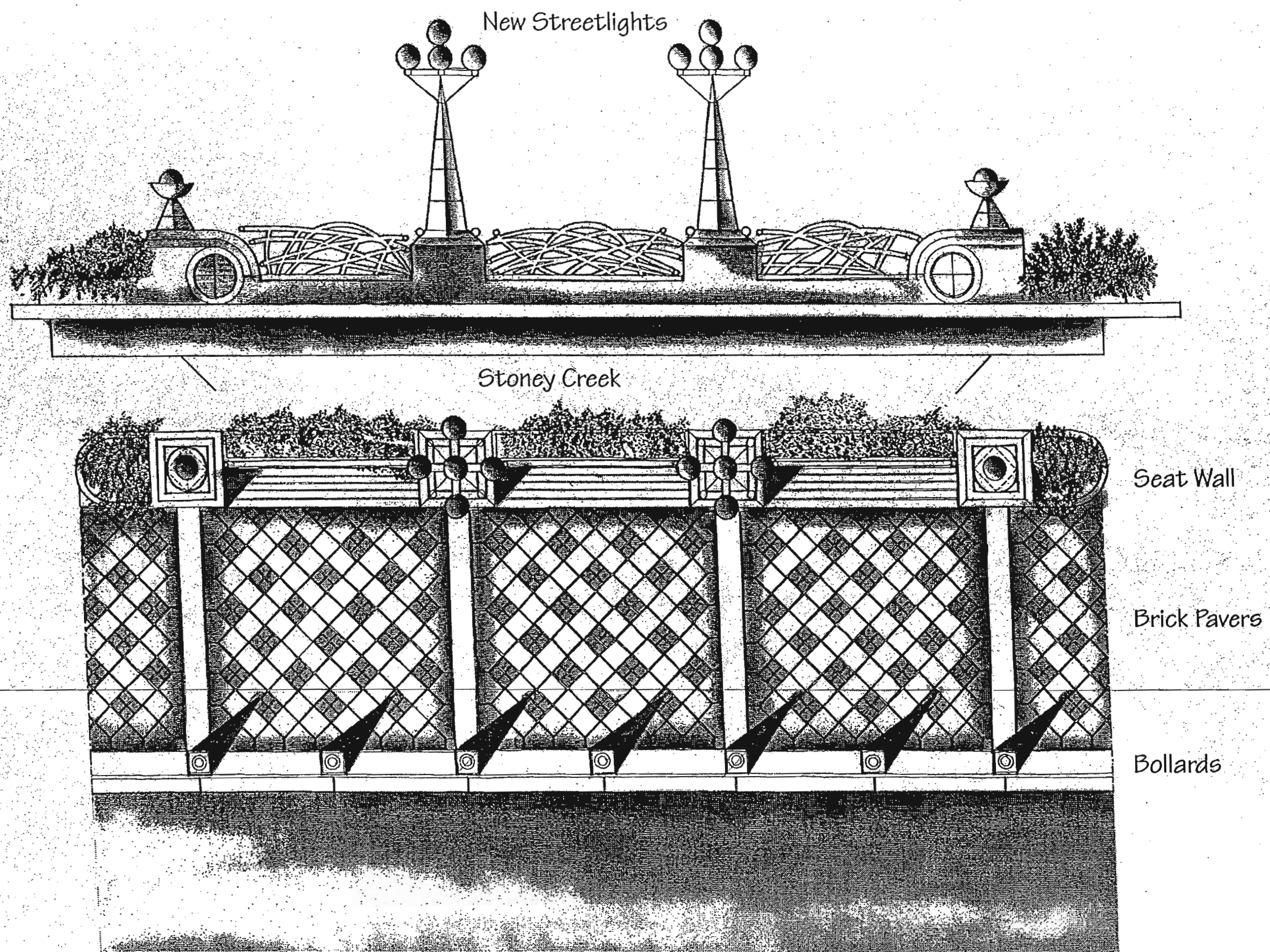
Perspective drawing displaying conceptual design of the Stoney Creek Bridge, crosswalk improvements at Elm Ave. and built-form principles.



Stoney Creek Bridge at King St., south side

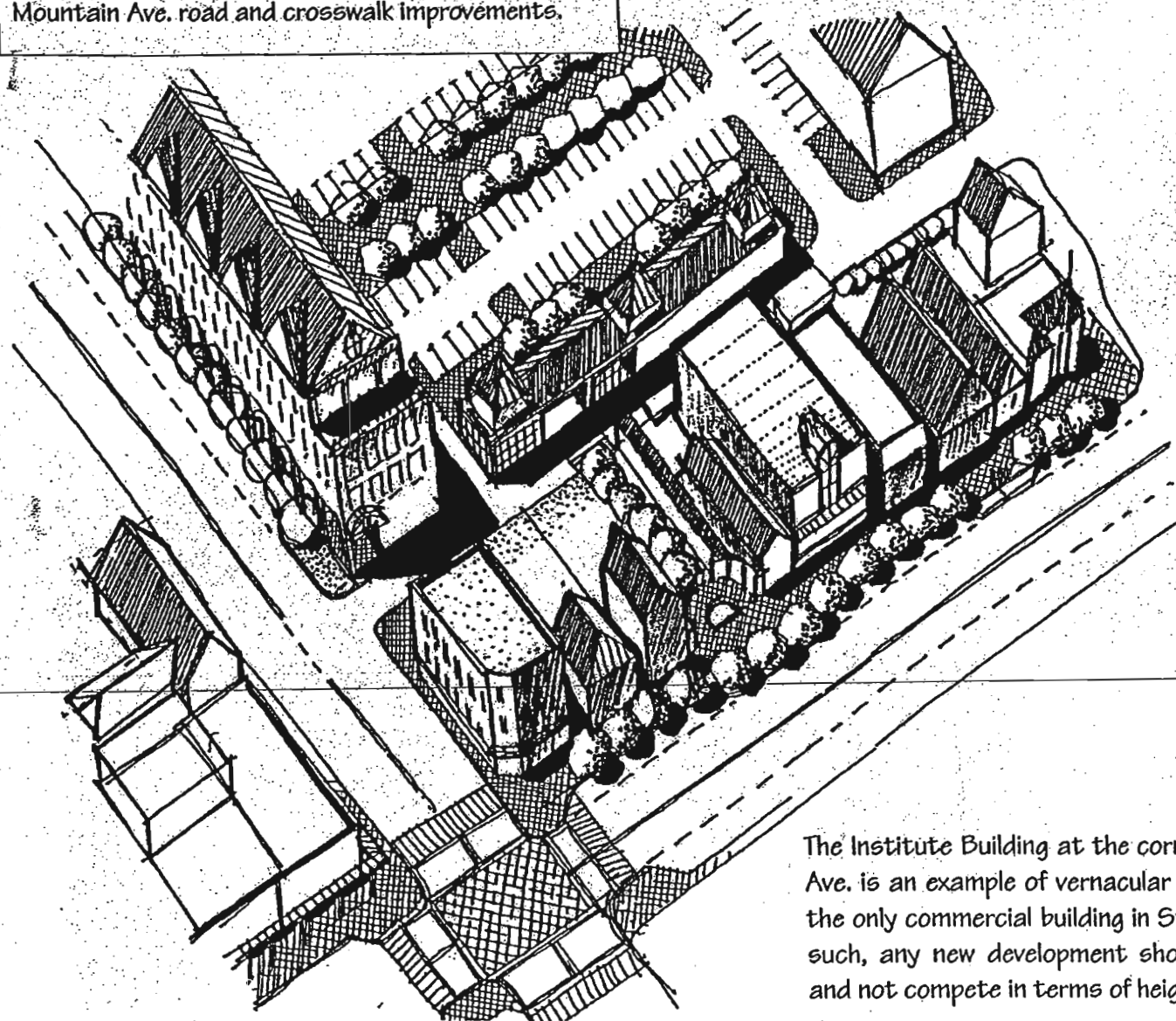


Stoney Creek Bridge at King St., north side



In conjunction with the Olde Town Centre Project, a joint private/public venture, a number of physical improvements are required. They include buildings designed in accordance with specified principles, maintaining surface parking which allows a market as a secondary use, a network of landscaped links with street furniture in the parking area, a pedestrian/service lane and landscaped features. Mountain Avenue is to be improved with new paving and curbing along with an covered (arcaded) sidewalk, landscaping, on-street parking and a widened eastern sidewalk.

Perspective drawing displaying conceptual design of the Olde Towne Centre Project, Market Area and the Mountain Ave. road and crosswalk improvements.



The Institute Building at the corner of King St. and Mountain Ave. is an example of vernacular Edwardian Classicism and is the only commercial building in Stoney Creek of this style. As such, any new development should compliment this building and not compete in terms of height.

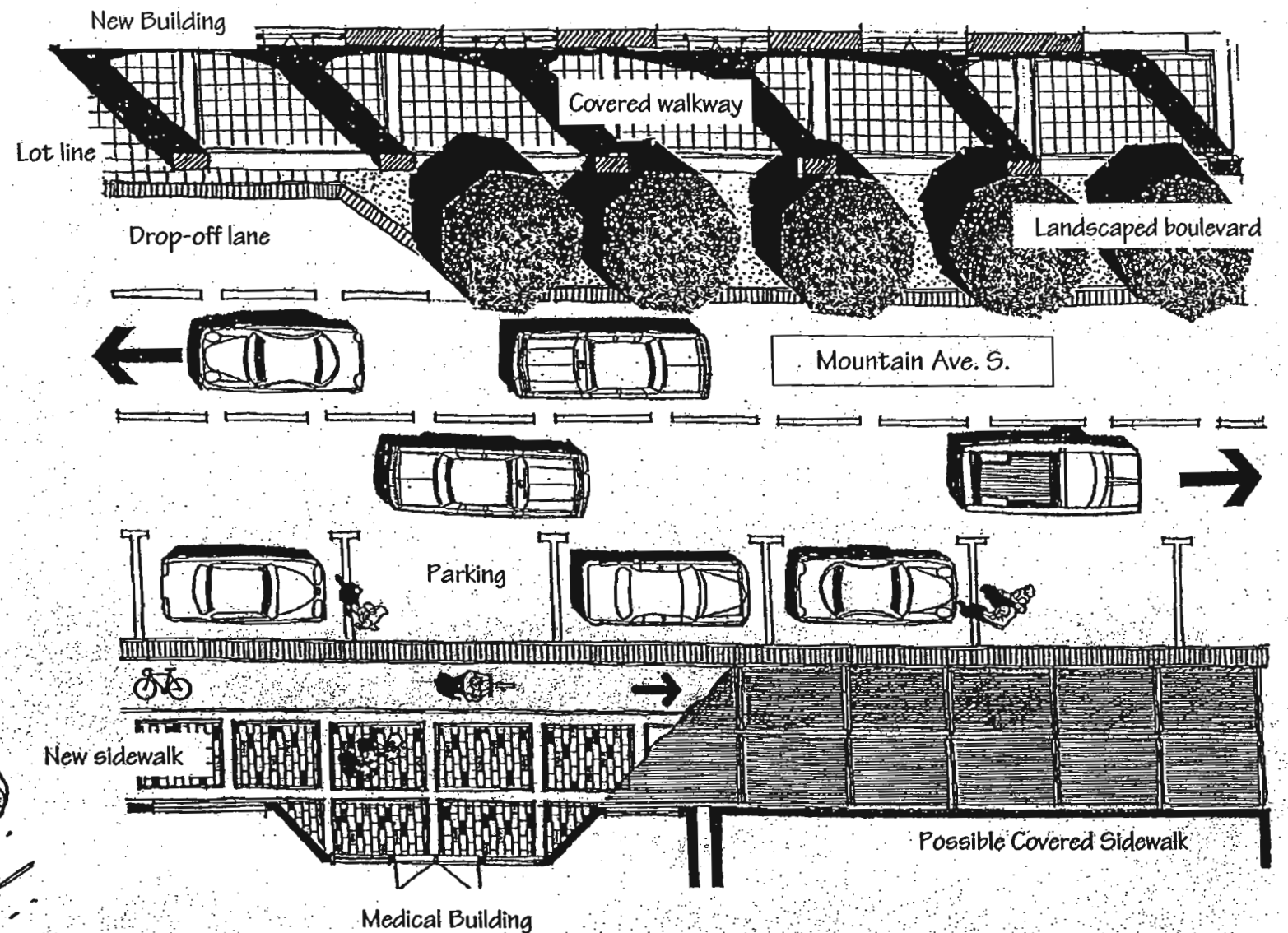


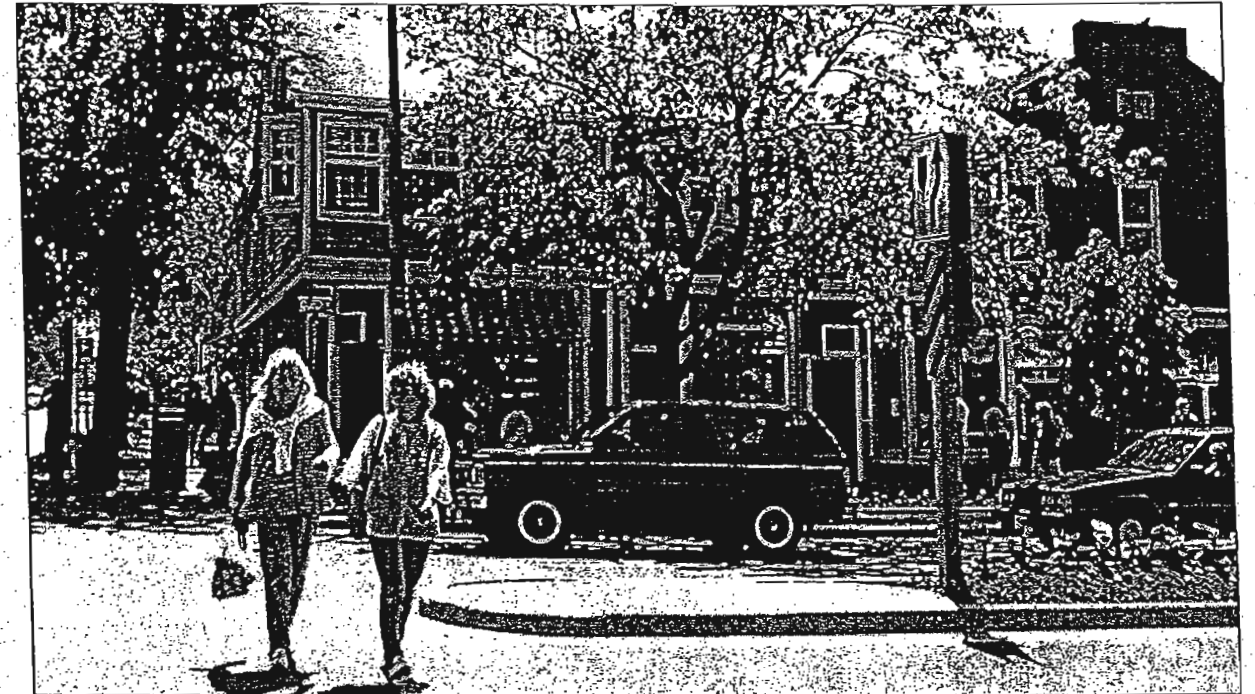
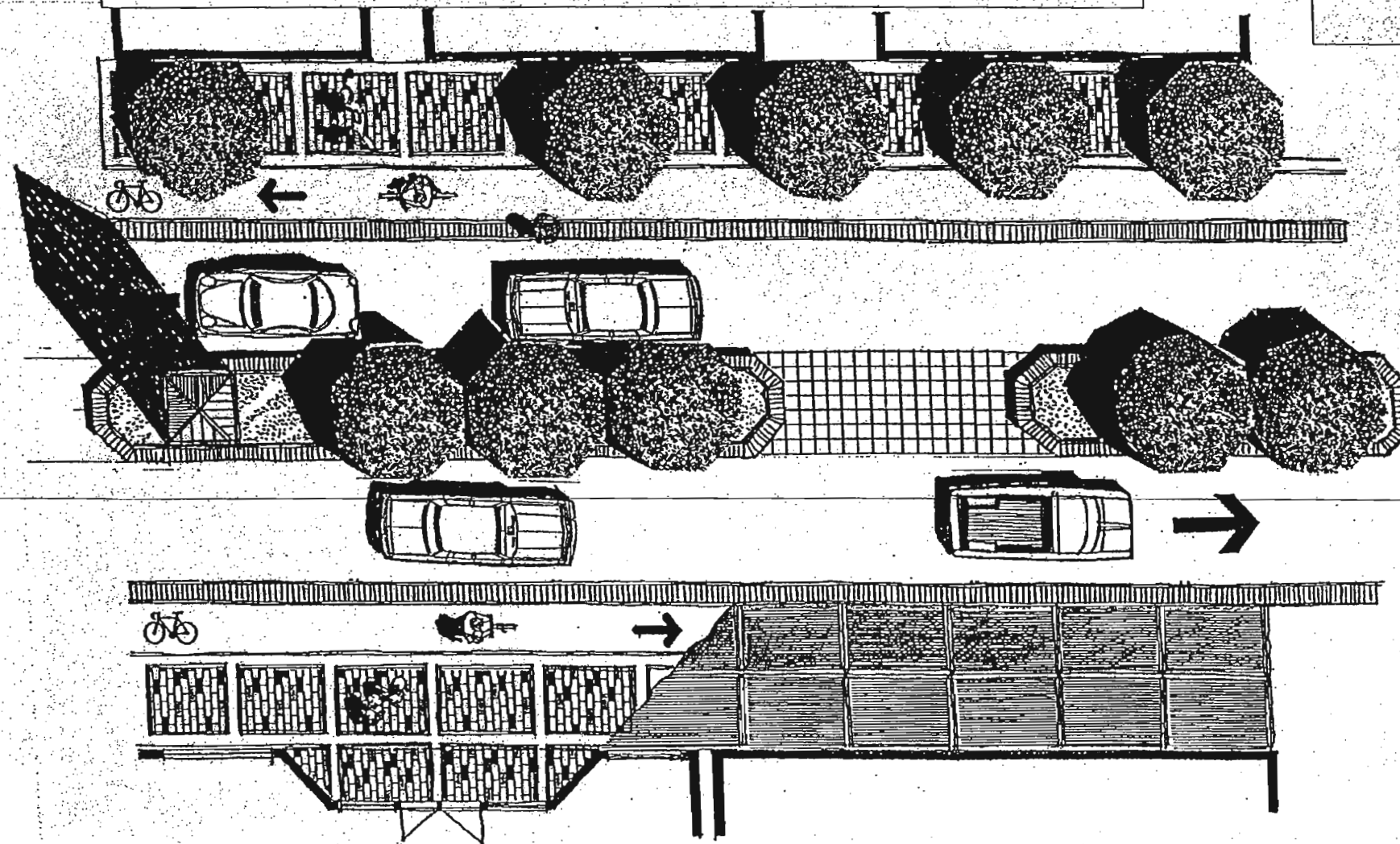
Image displaying design and built-form concepts for new development on Mountain Ave.



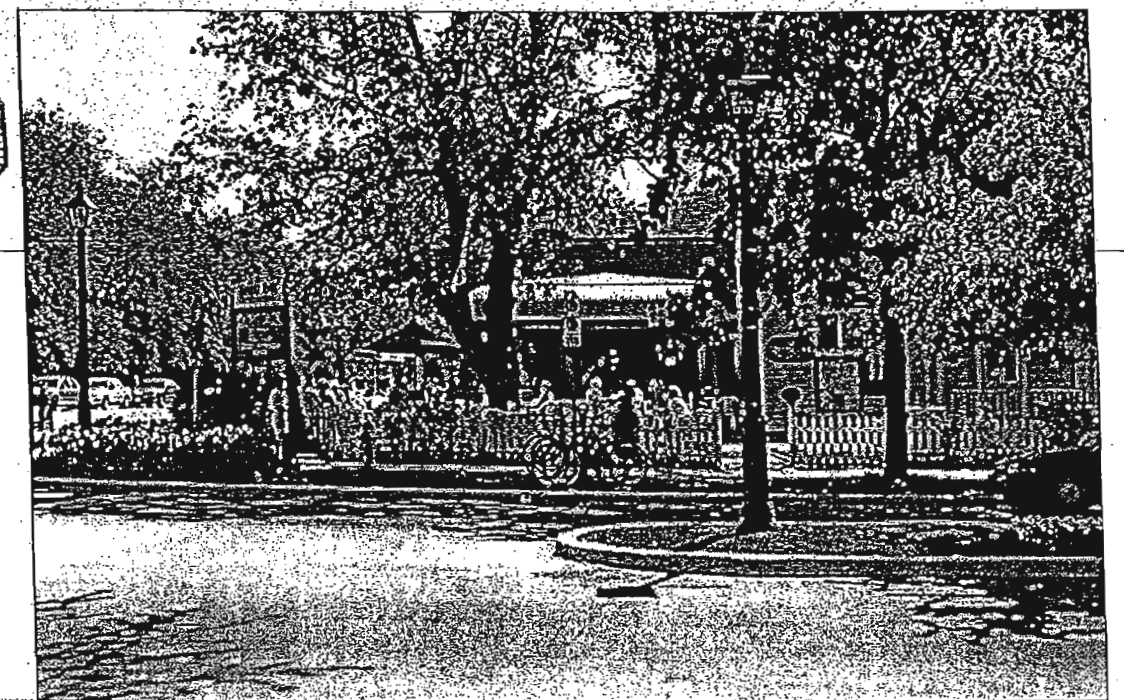
The proposed boulevard/median will create a westerly gateway feature for the Downtown Core to denote arrival into a distinct area and to create a visual impression. The proposed boulevard should be approximately 2 to 3 metres wide in order to incorporate deciduous tree planting, a flower bed and to have the appropriate visual effect. It is also suggested that architectural features be included such as a tower or flag poles.

The boulevard should extend from the downtown core in the vicinity of First Street to Second Street. The potential also exists to extend it to Community Park.

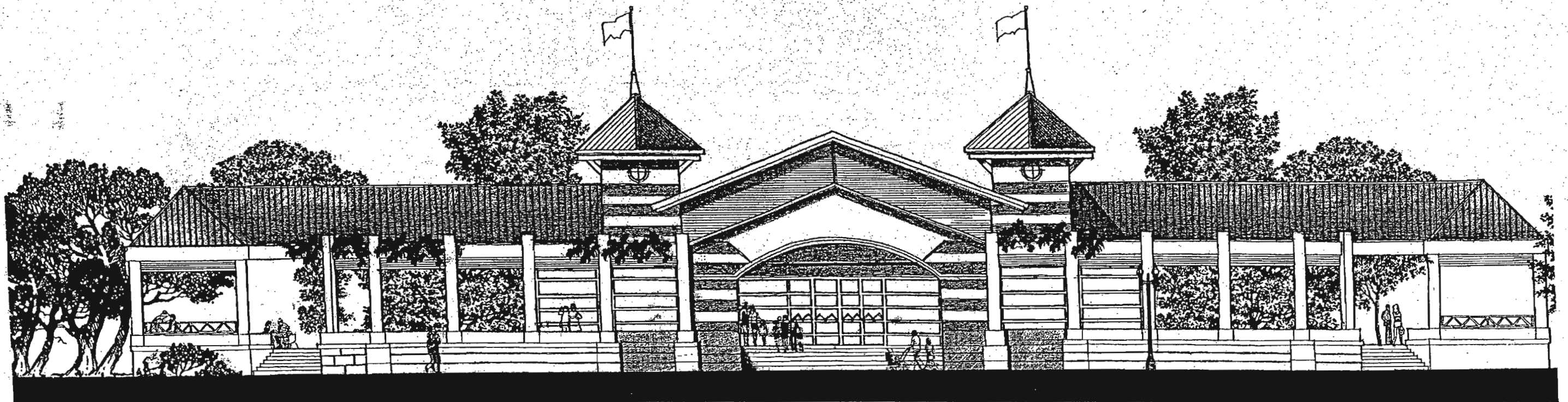
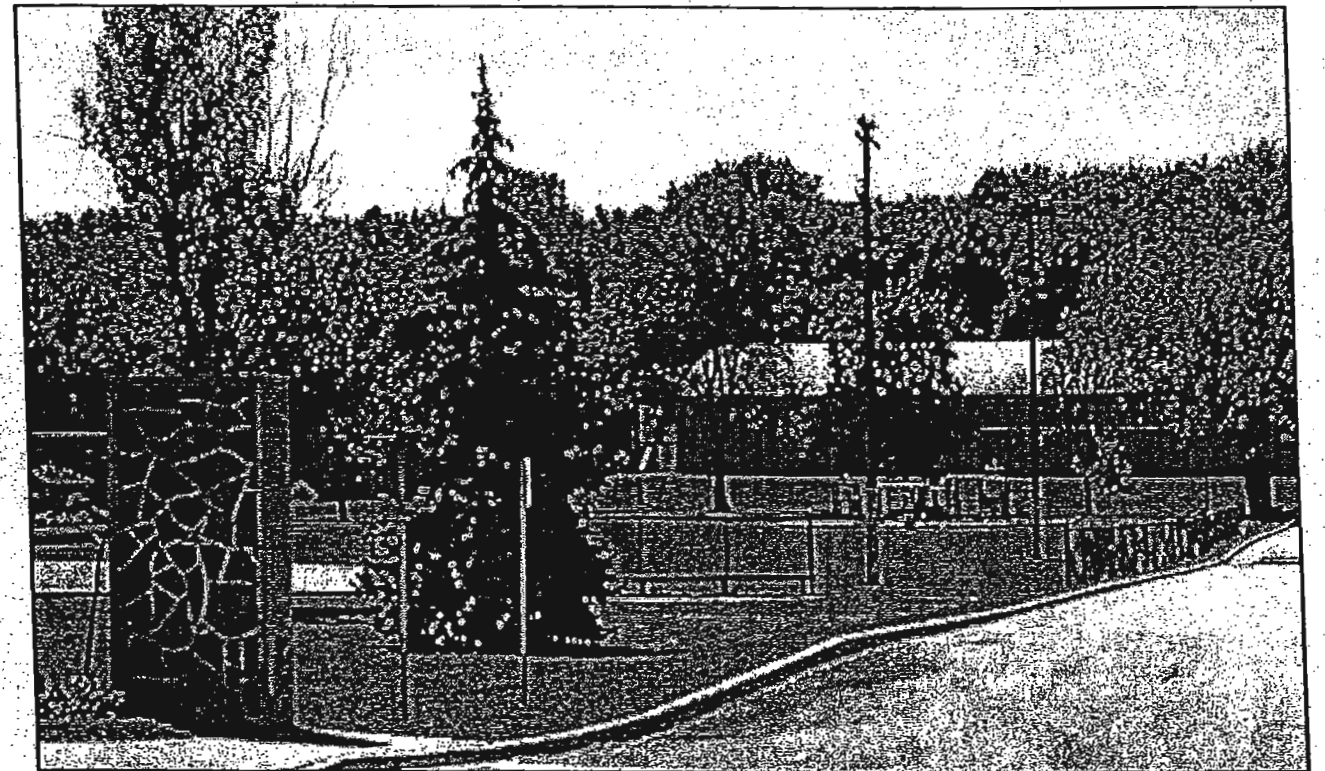
Perspective drawing displaying conceptual design of the King St. Boulevard at Second St.



Examples of landscaped boulevard/medians and the urban and functional character they impart on a street.

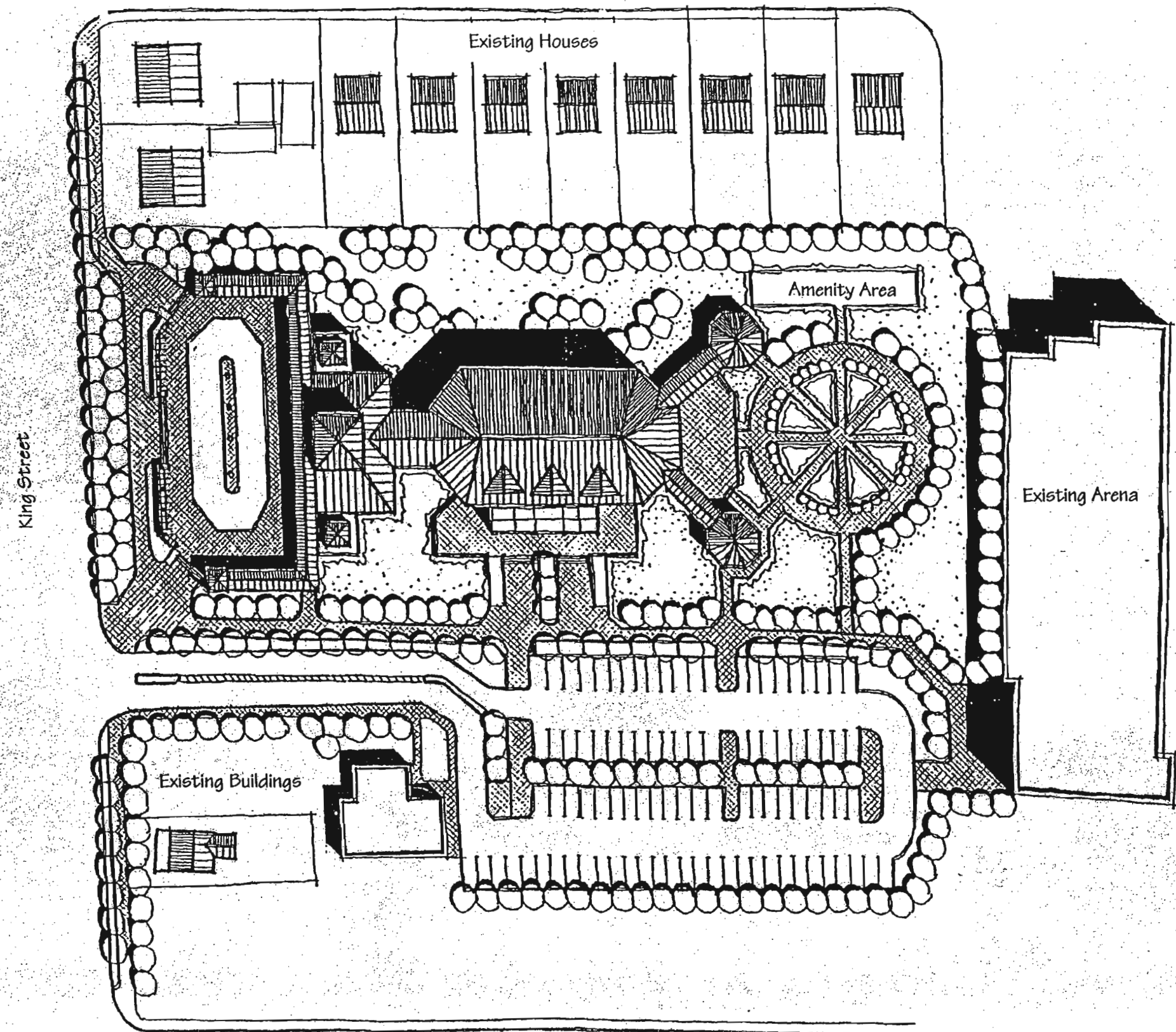


Removal of the baseball diamond from Community Park in favour of a more passive open space use which draws people to the park is required. A Seniors' Centre Complex is a very desirable component to the site as it is a public generator and is augmented by the open space function and the existing parking facility.



King Street

The design concept for the Community Park Complex includes a public space adjacent to King Street for use as a summer square and winter outdoor ice rink. Both uses would compliment the existing arena and summer program functions of the park. Provision is made for small commercial functions associated to the public space and Senior's Centre. The architecture can be contemporary or post modern and should be articulated to maximize "openness" and solar exposure.



As part of the Urban Design Plan, a formal plan for this site is being prepared in order to enhance the site to a prominence befitting a landmark of national significance. Elements of the site plan include removal of part of the front stone wall to open views from King Street and access via steps from a widened sidewalk. The smaller cannons will be relocated to a more prominent and visible location along the street line while the larger cannons will be placed beside the Memorial. Also improved is on-site landscaping, surface treatment with brick pavers and illumination together with the introduction of park furniture. The American Memorial is also to be formalized while the Memorial itself should be restored. A high quality pedestrian crossing is also suggested at Battlefield Drive with design elements to slow traffic in front of the site and to connect it with Battlefield Park.

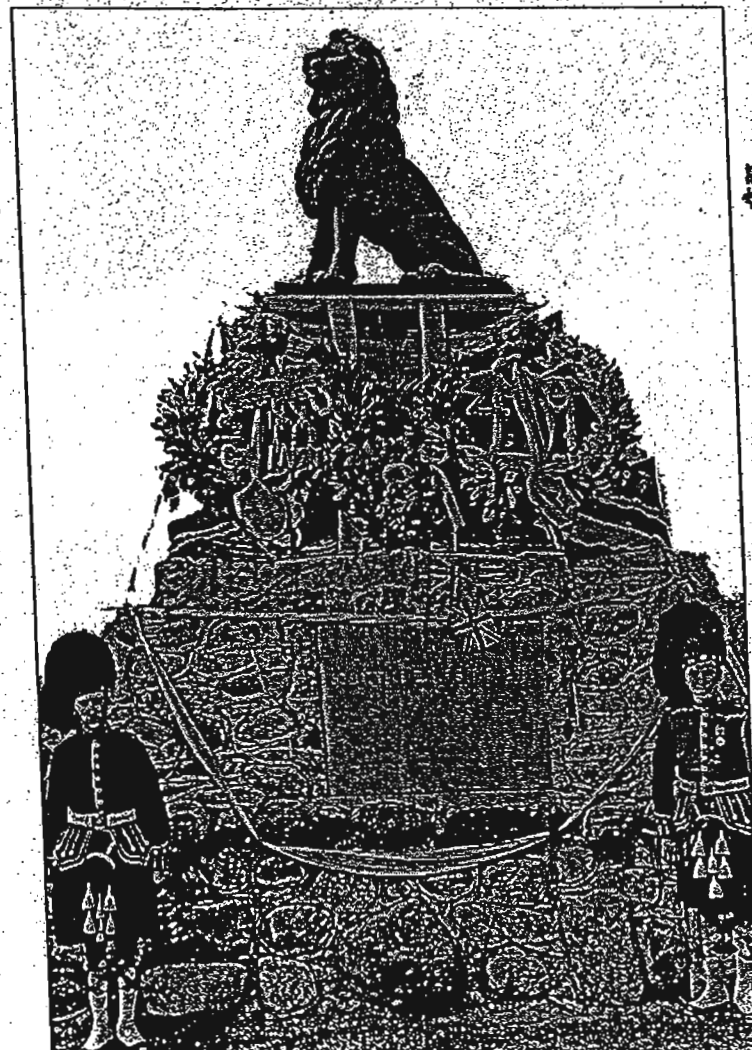
History - Cemetery Memorial

Some of those who had fallen during the attack on the American guns during the Battle of Stoney Creek were interred in Smith's Knoll where they had fallen.

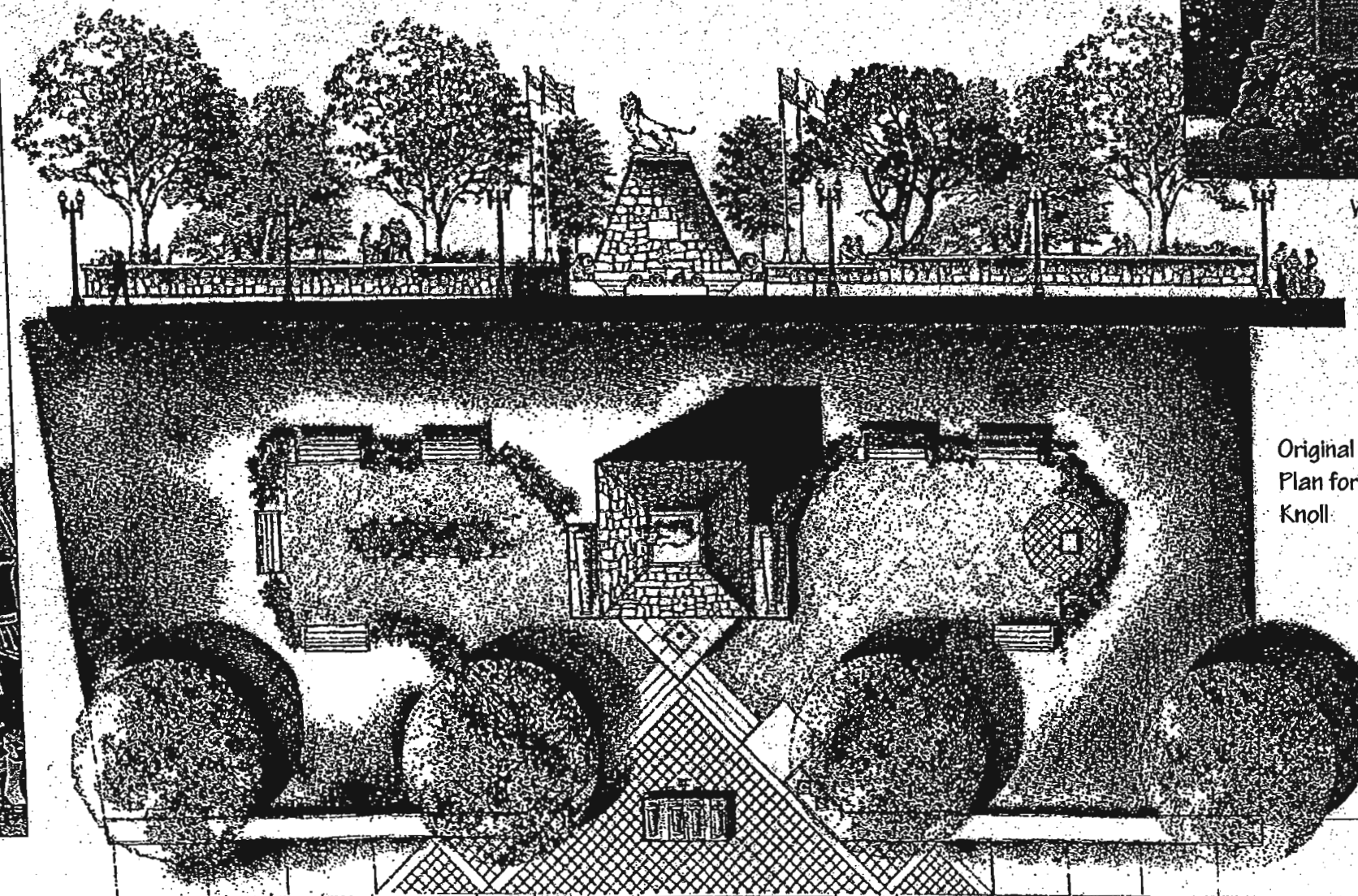
The monument was built around 1910 and consists of a 4.0 metre high truncated pyramid constructed of uncoursed fieldstone laid in cement mortar. There are a number of authenticated cannons on site.



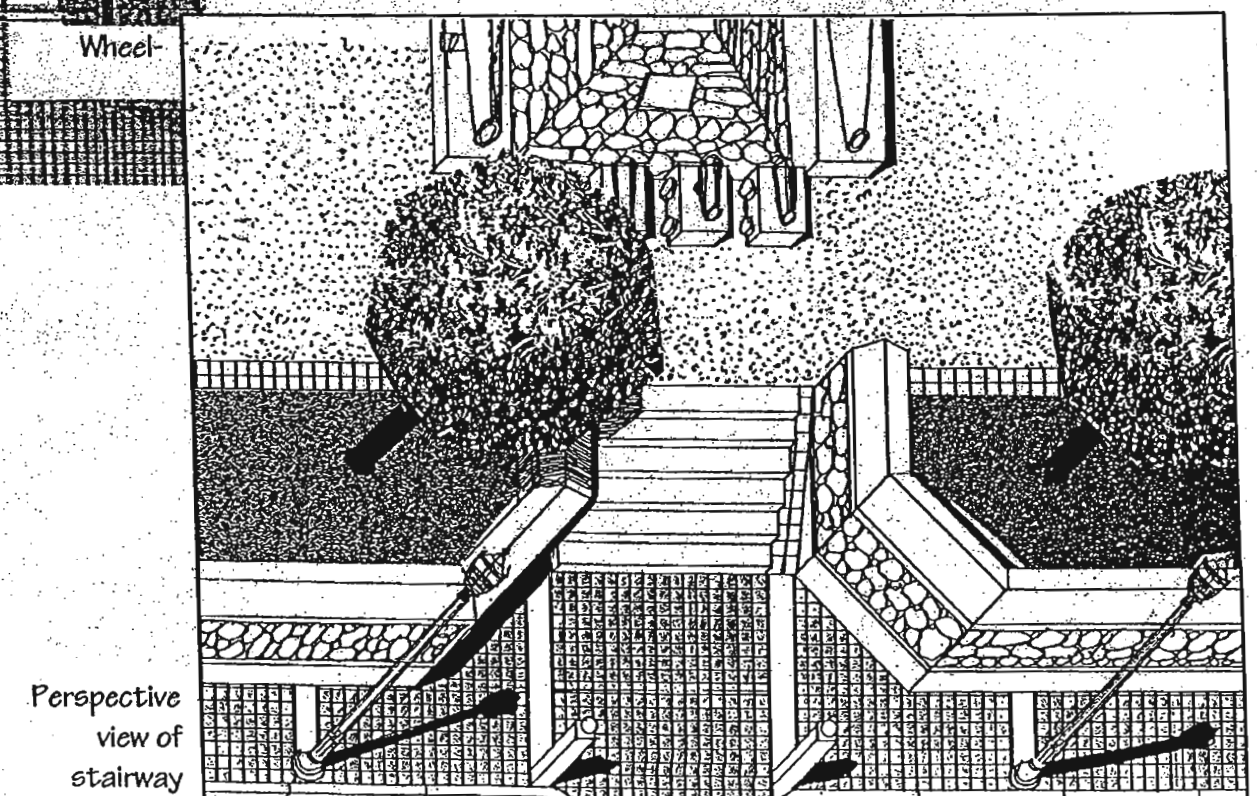
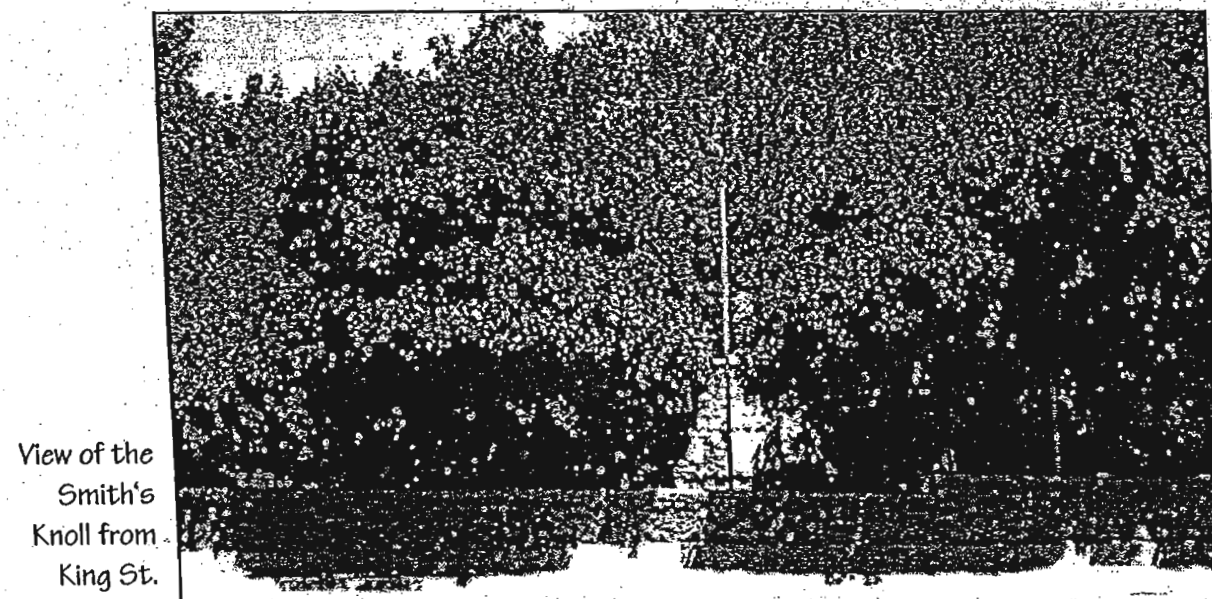
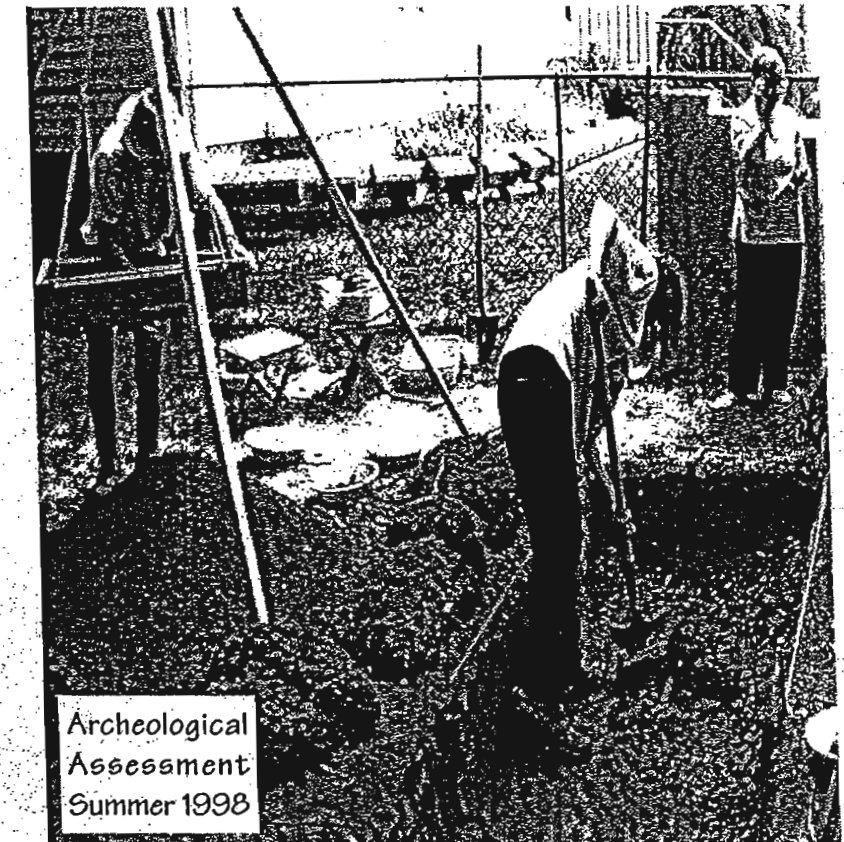
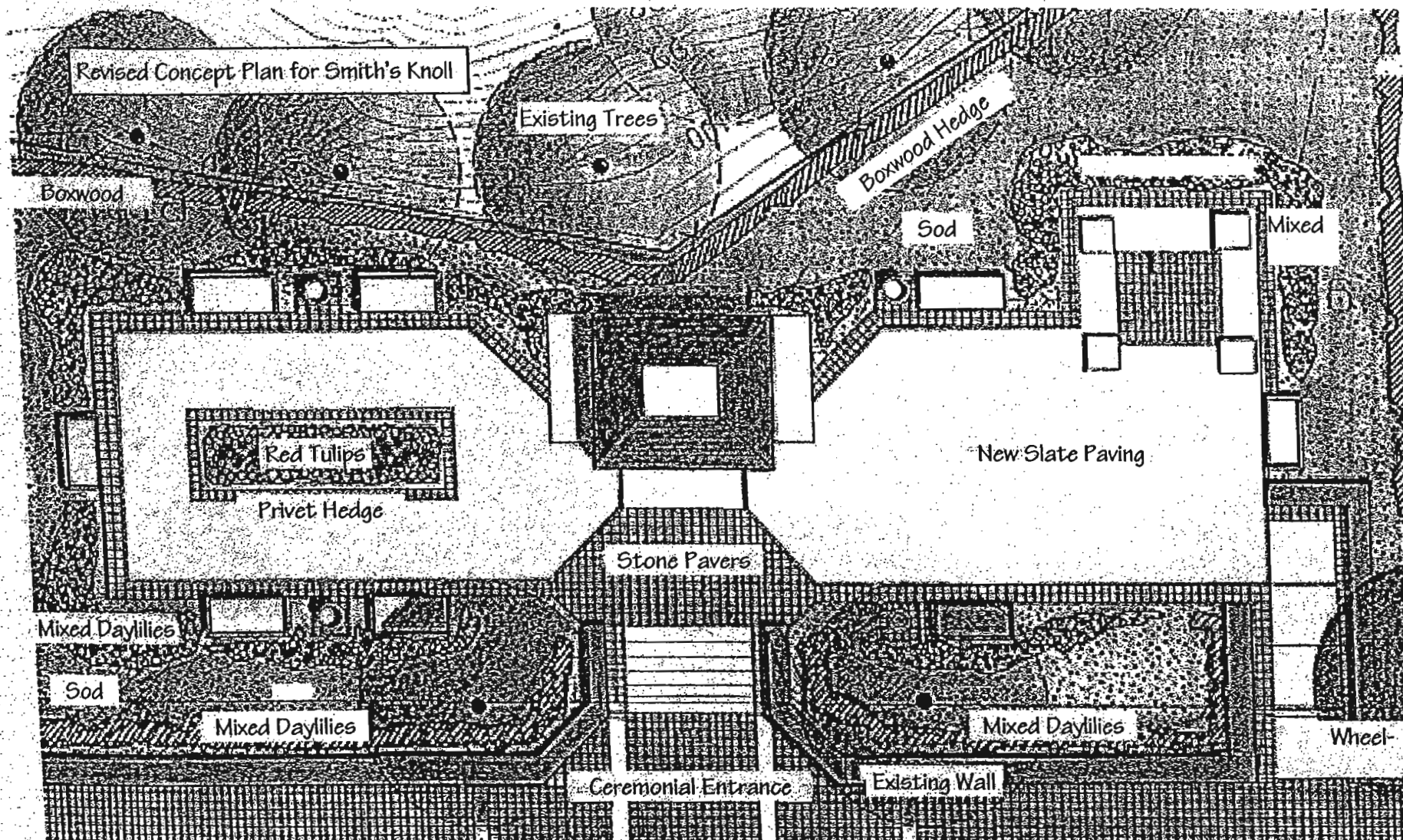
View of the "Lion's Monument", 1997



Dedication of the "Lion's Monument", c. 1900



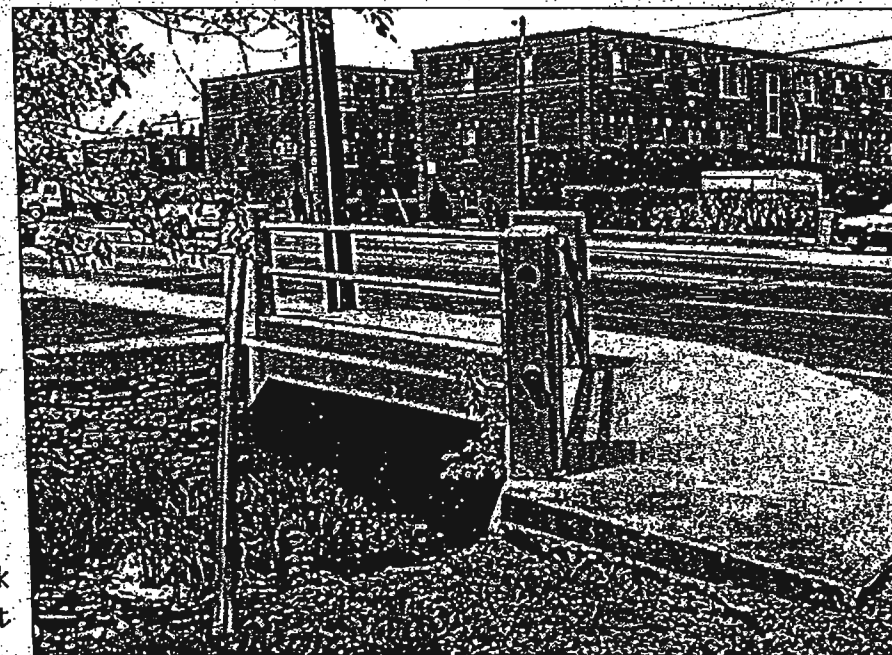
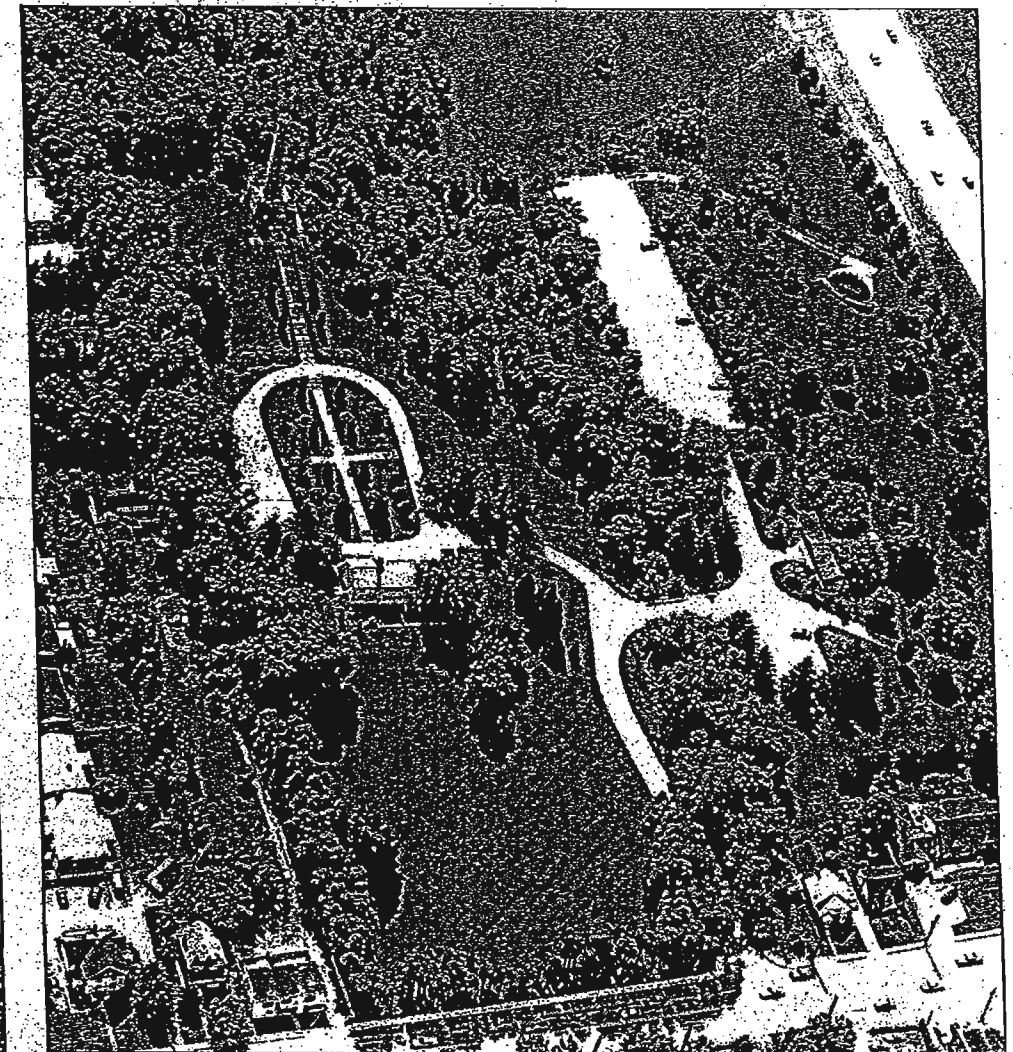
Original Concept Plan for Smith's Knoll



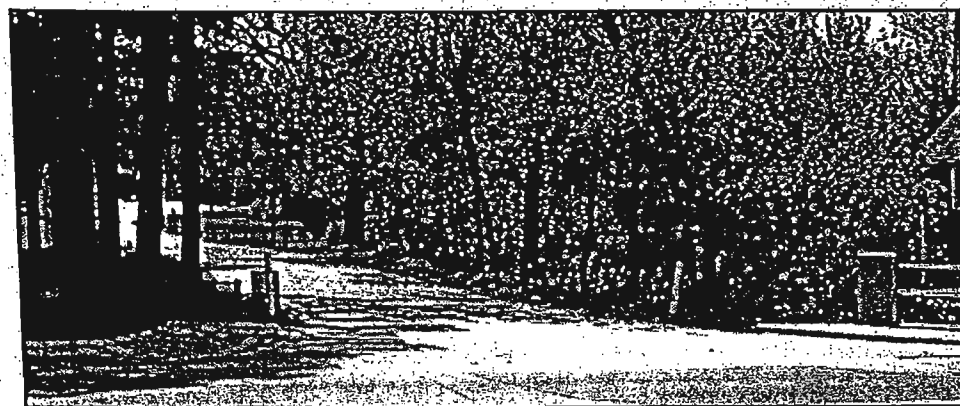
In conjunction with the Battlefield Park Master Plan, it is necessary to expedite the property acquisition of the remaining properties on King Street in order to relocate the park entrance and to enhance the views to the Park as well as the planned development of the interpretative centre. Also recommended is the relocation of the sidewalk along King Street 2 metres to the south and through the park and to establish a colorful landscaping adjacent to the curb. Also required is the introduction of park furnishings throughout particularly along the Battlefield Creek and along King Street. The Battlefield Creek Bridge should also to be embellished in conjunction with the entrance reconstruction. It is suggested that the Bridge have strong historical detailing to compliment Battlefield House.

The Stoney Creek Battlefield Site and its environs is the most significant historical site in Stoney Creek. The entire site consisting of the Battlefield House, the Monument and Park are protected under Provincial Legislation. A landscape plan developed for the park early this century by the famous Landscape Architect firm of Dunnington & Grubb was never implemented. The Master Plan for the park has more recently been prepared and adopted for the park to address design, use and contextual issues.

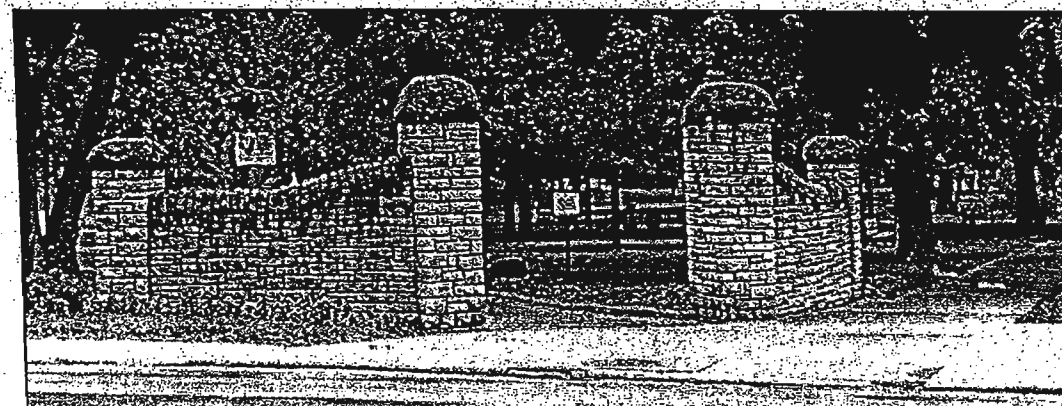
Aerial view of Battlefield Park



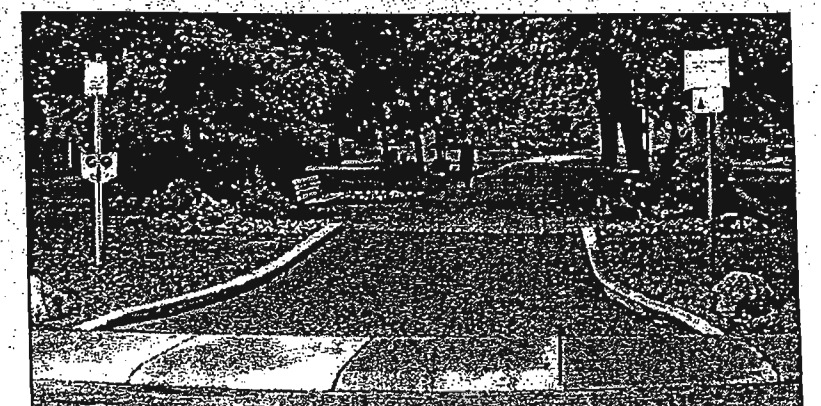
Battlefield Creek Bridge at King Street

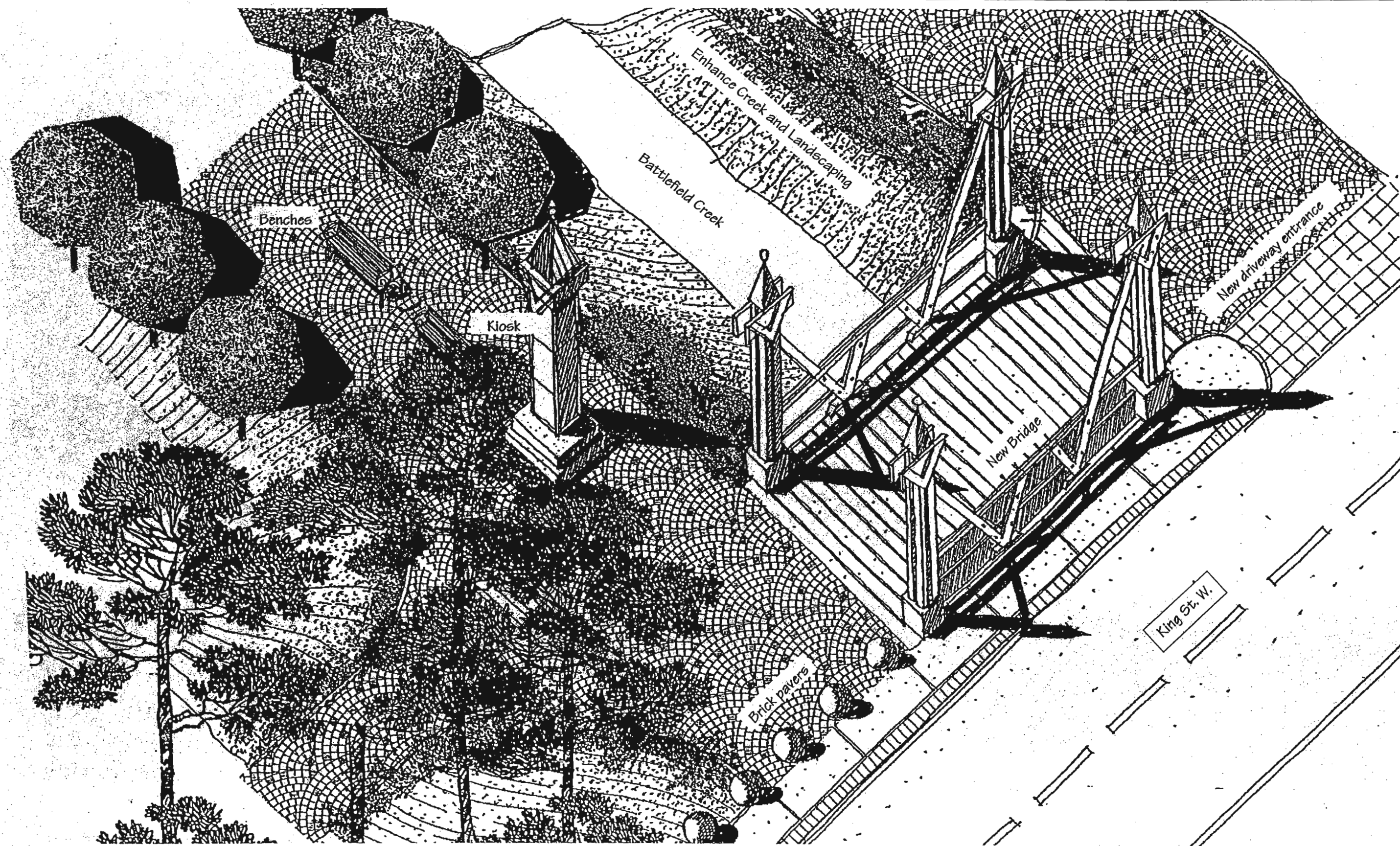


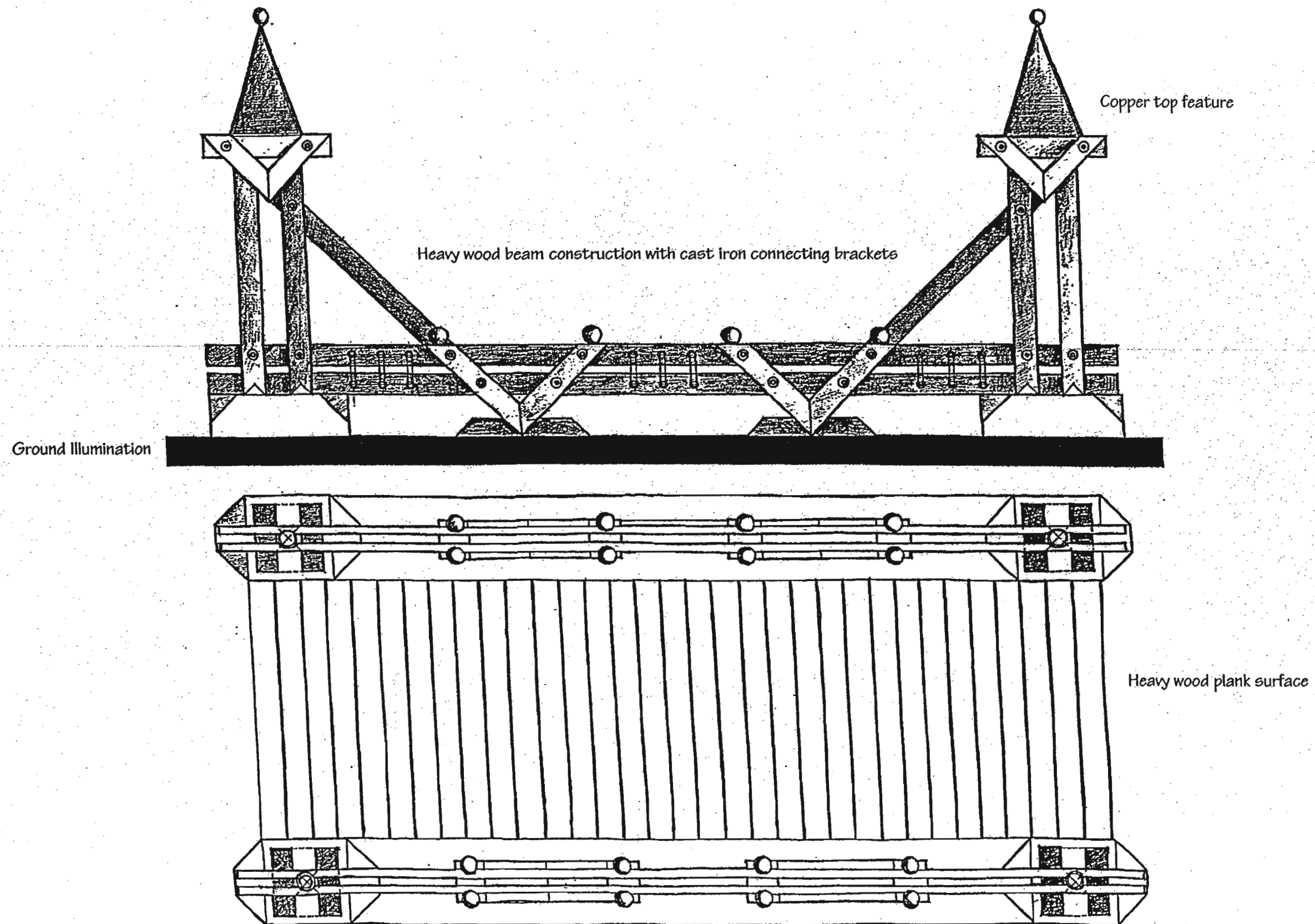
Existing pedestrian/vehicular entrance to Battlefield Park



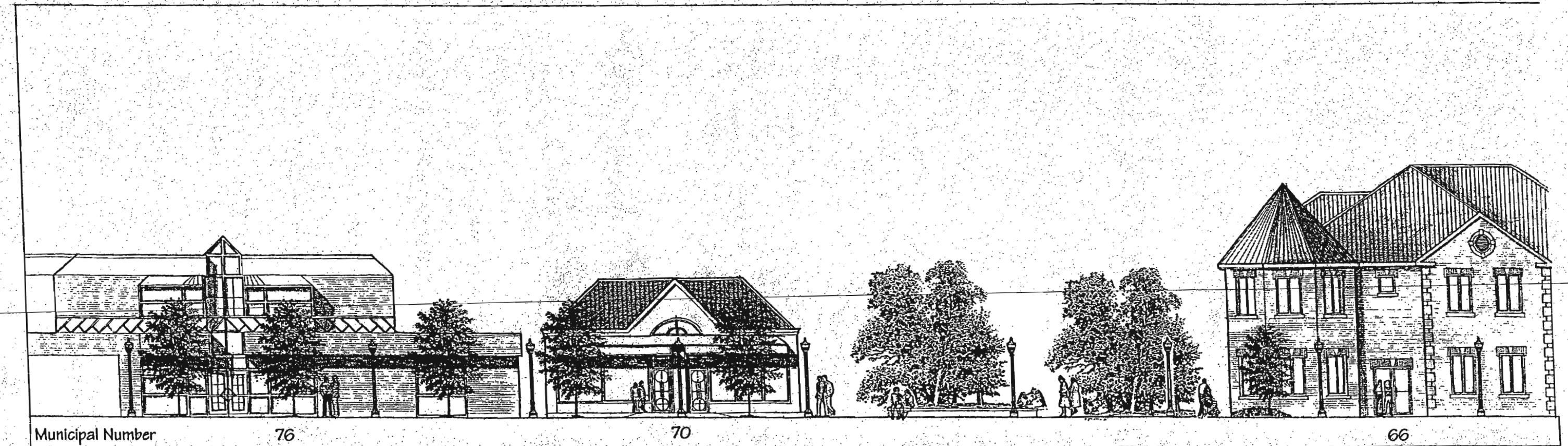
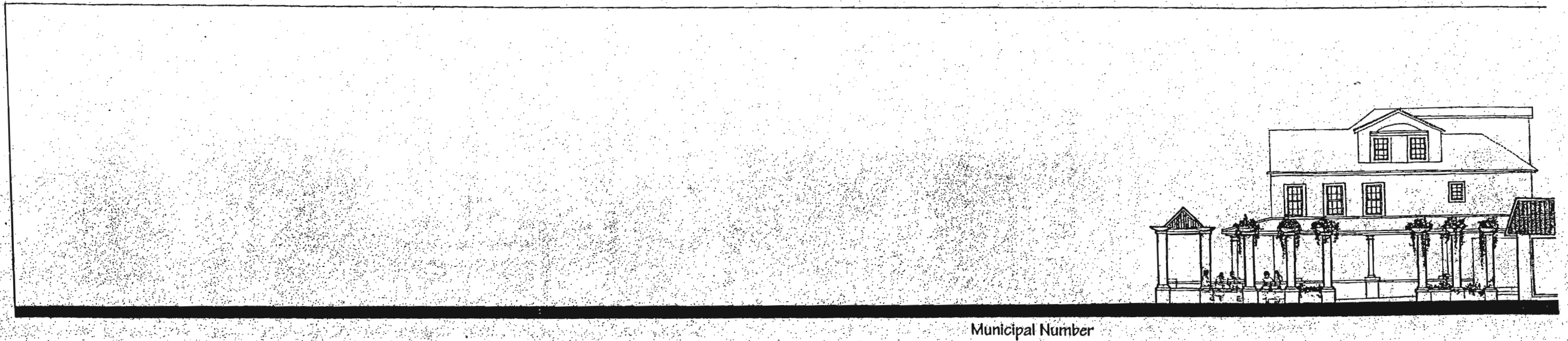
Examples of pedestrian and vehicular entrance features and landscaping for Battlefield Park



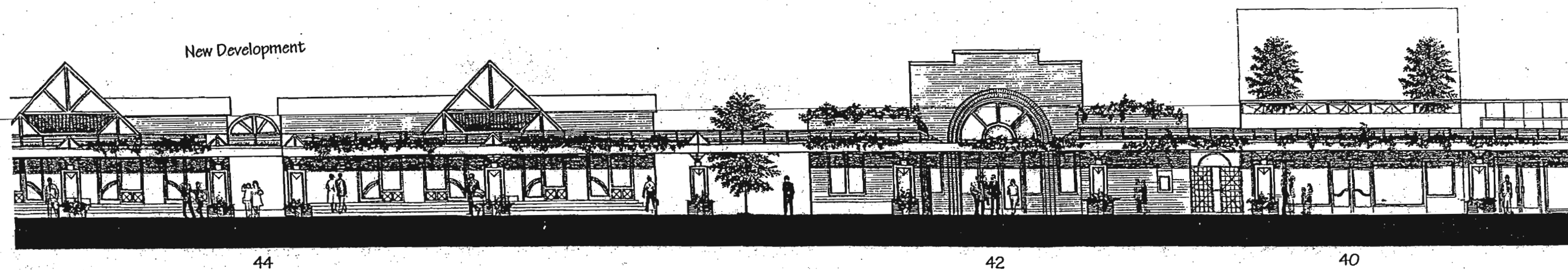


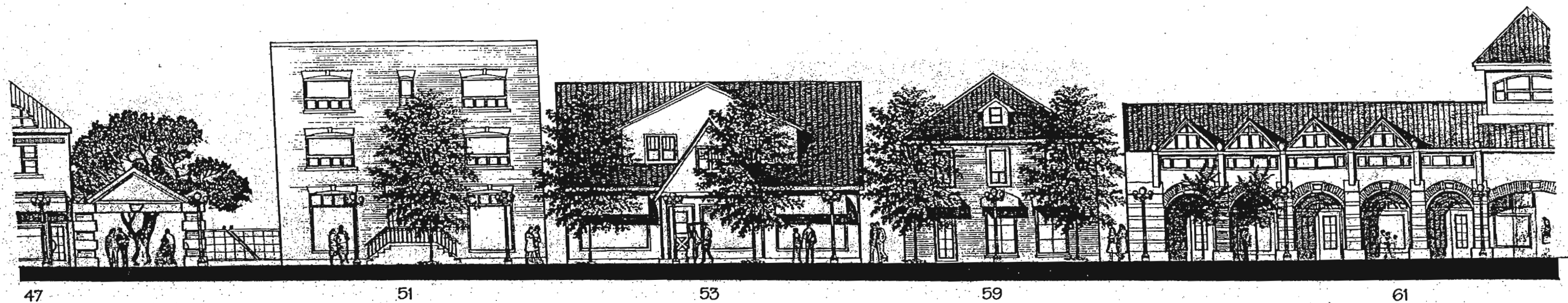


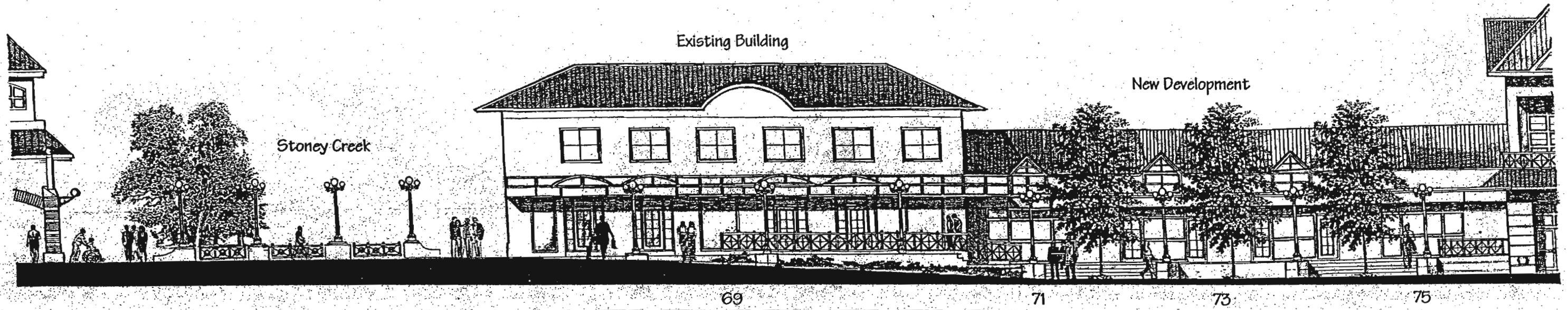
The purpose and role of the following elevations is to interpret graphically the urban design principles expressed by this Plan. They also illustrate the urban design vision by establishing a common "look" to guide future actions and investment by both the private and public sectors. They strive to create harmony among the built-form while at the same time expressing a uniqueness and excellence in design.





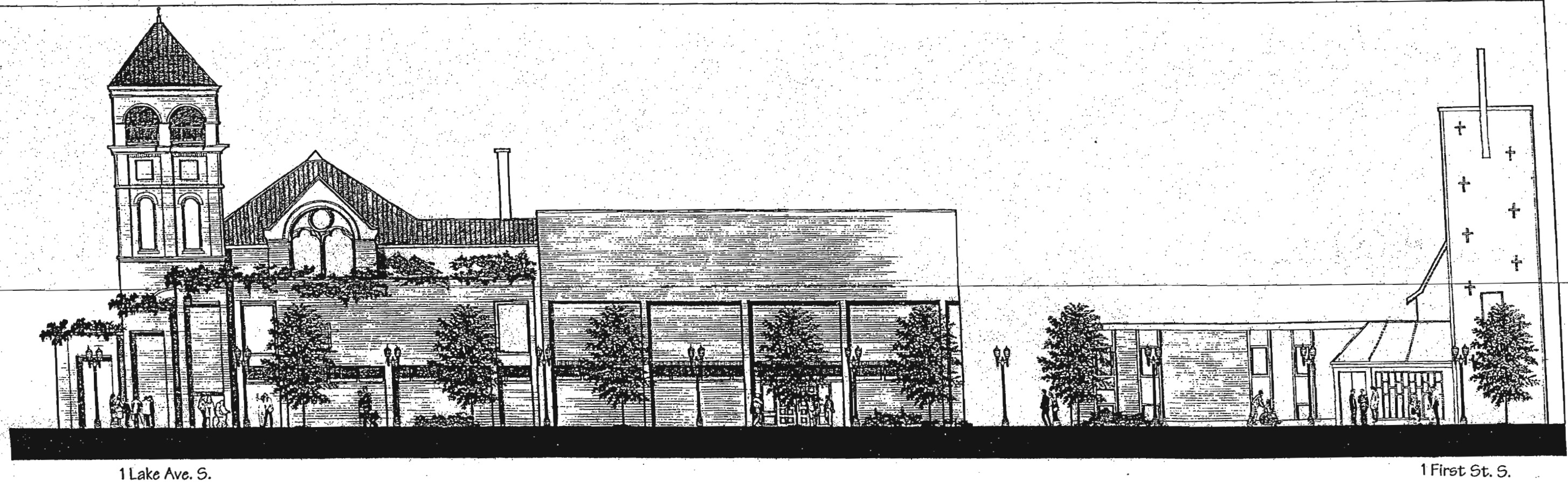
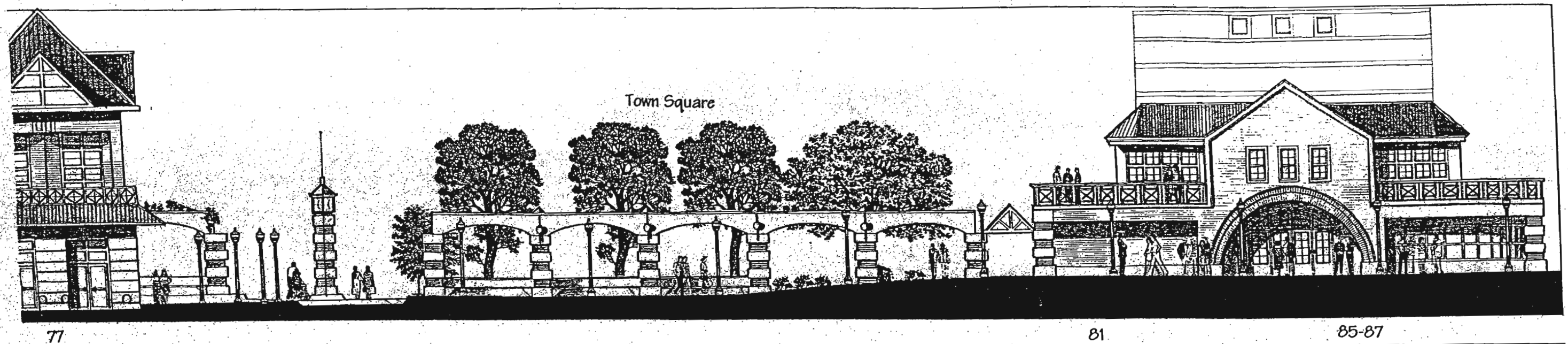


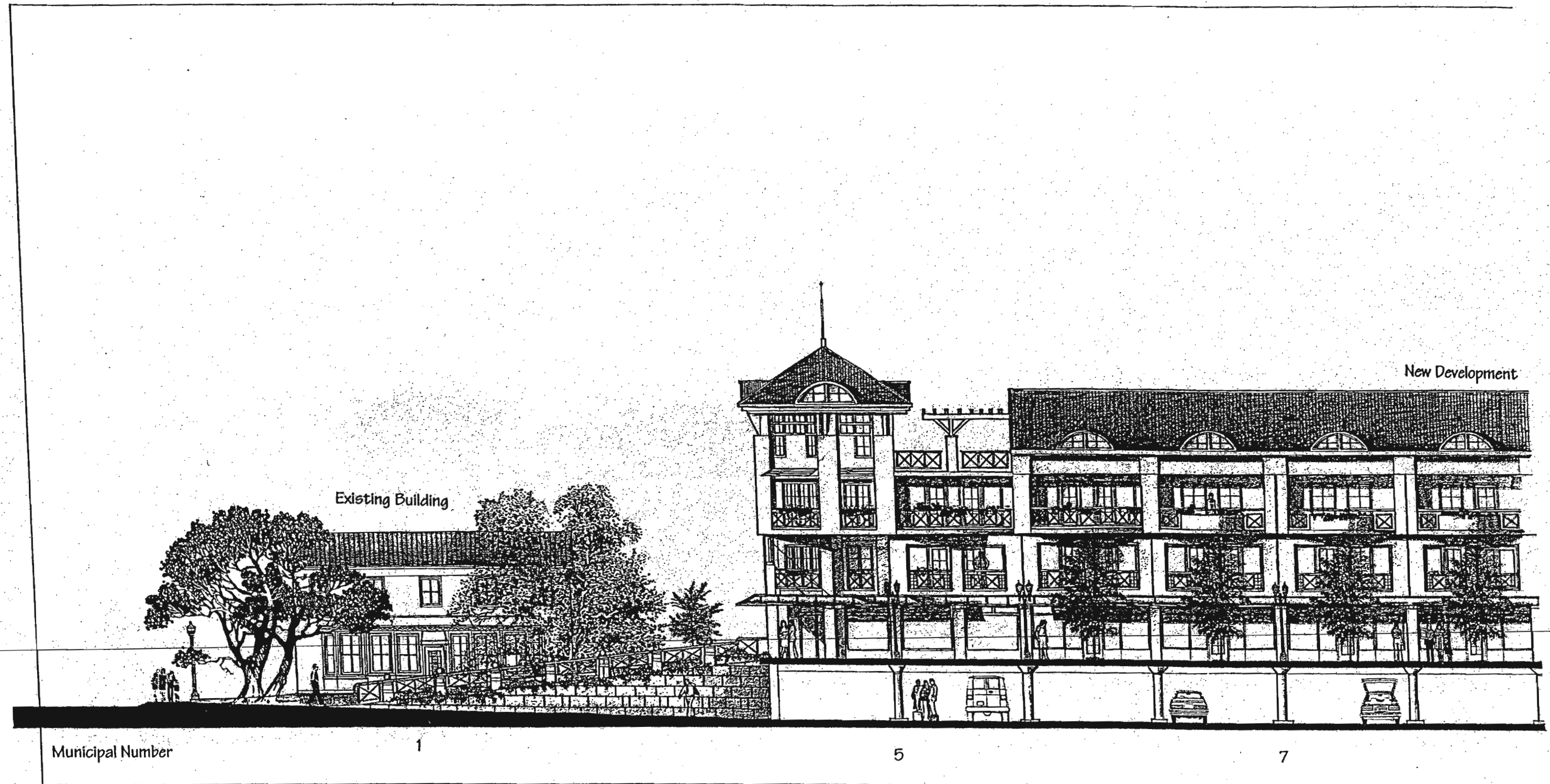




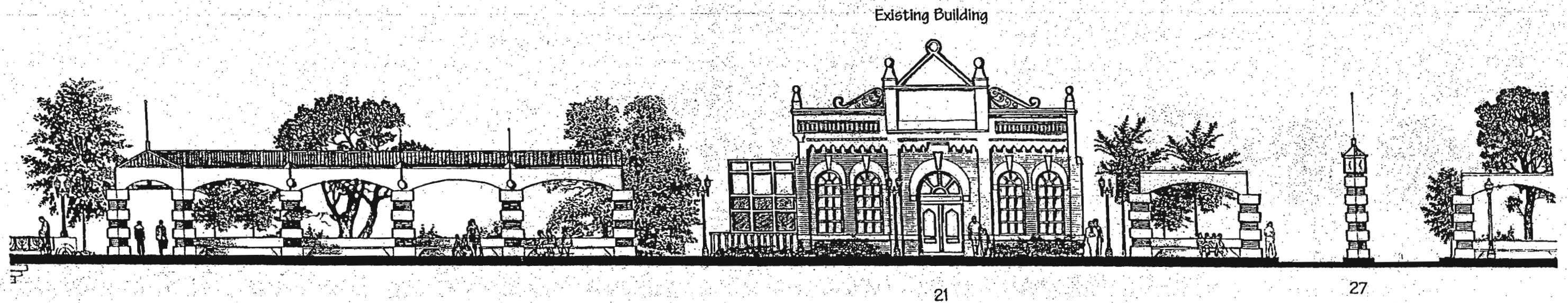
6.1 KING STREET ELEVATIONS - NORTH (Top), SOUTH (Bottom)

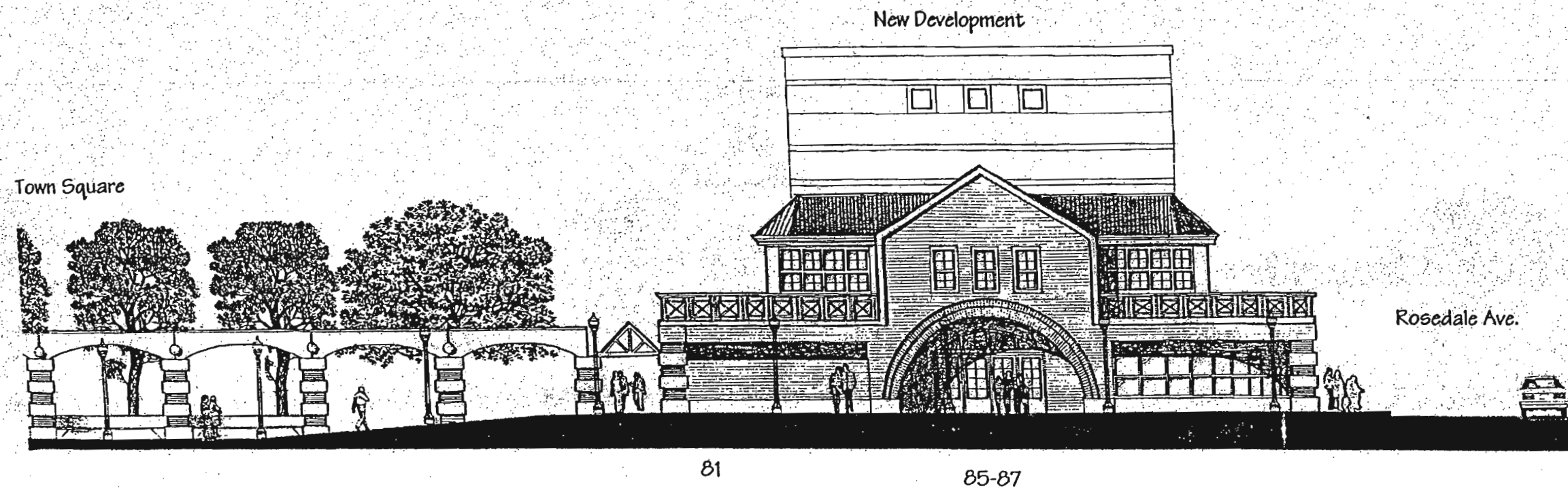
Page 46

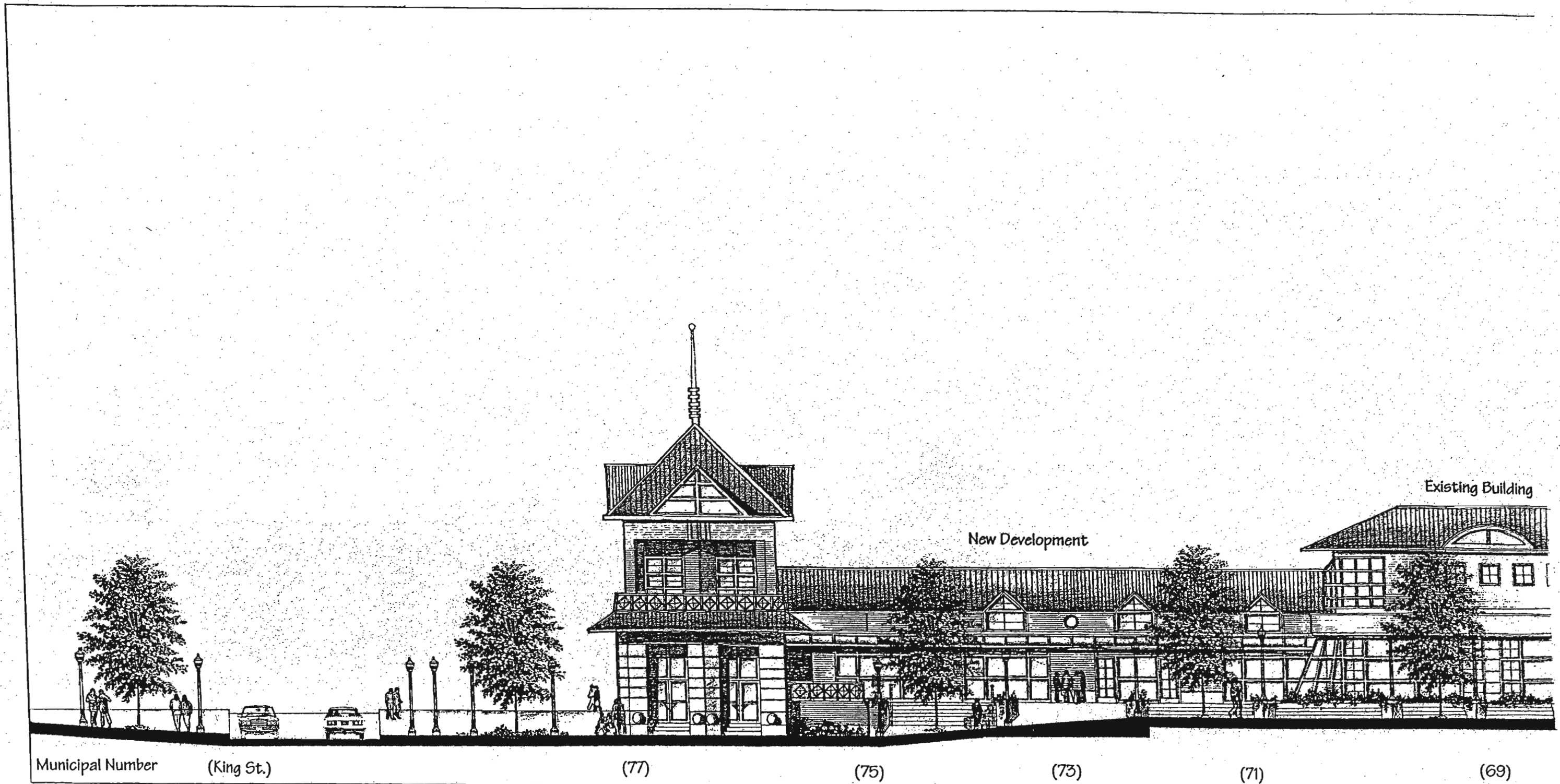


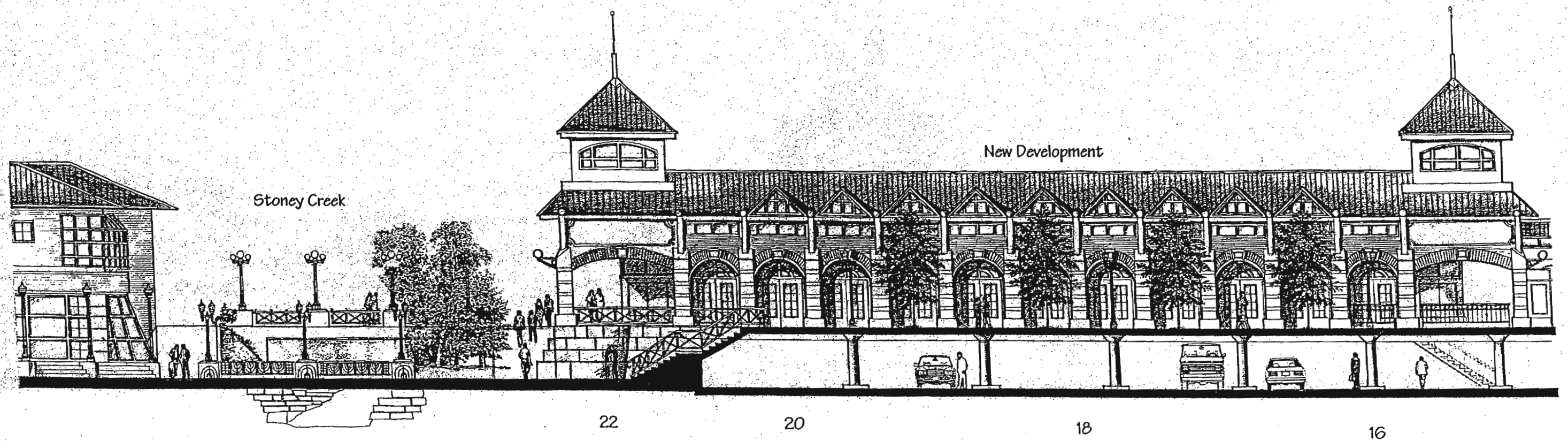


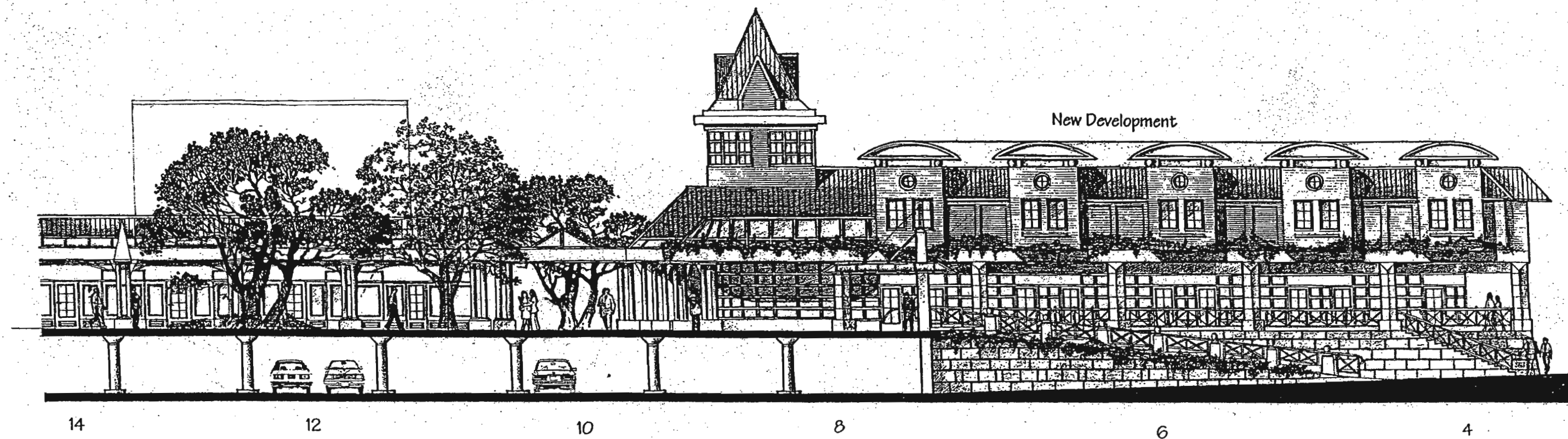


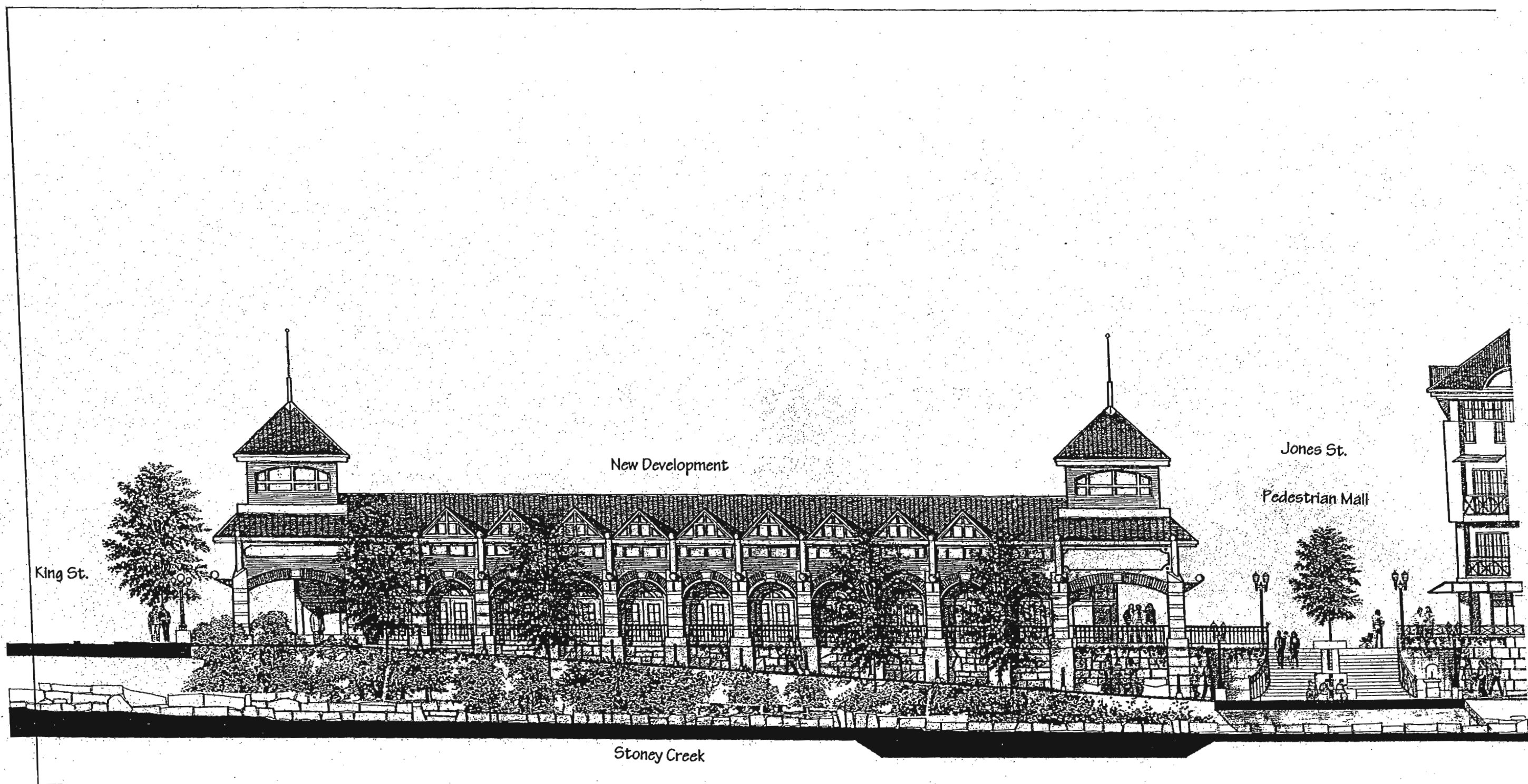


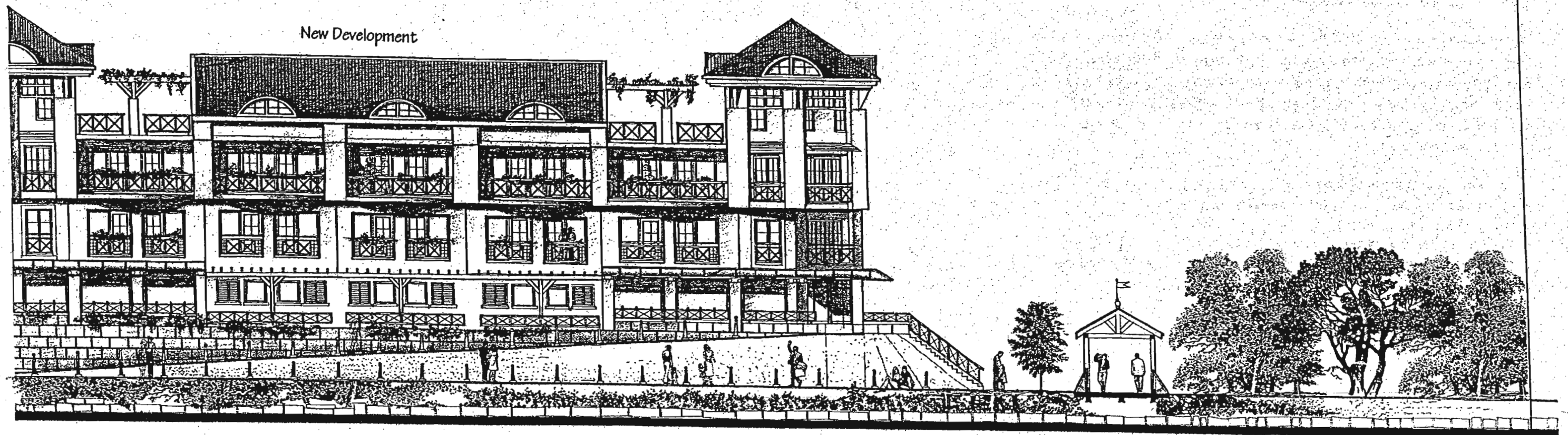












The public elements of this Plan are to be implemented over 15 years (1998 to 2012) to coincide with the Bicentennial of the Battle of Stoney Creek (June 2013). This Plan identifies a number of specific projects and suggests scheduling on the following time periods:

- Near Term Projects 1 to 3 years
- Medium Term Projects 4 to 9 years
- Long Term Projects 10 to 15 years

Proposed public undertakings are to be spread throughout the Plan area with priority initiatives directed for the Downtown Core (King Street - Jones Street). The intent is to develop quickly a range of highly visible, easily implementable projects in order to maximize early exposure and to gain momentum for the remainder of the Plan (bridges, streetscape, signage, curbing, lighting and parking).

Medium and long-term public undertakings would, over time, interconnect with the near term projects. They would also incorporate future infrastructure improvements that are planned but not yet built as well as private ventures. They also involve more challenging and costly facilities (Senior Centre, public squares and structures, access agreements, Battlefield Park).

The phased development of improvements should be co-ordinated with the proposed improvements by other public agencies.

It is anticipated that continued private sector development and restoration will continue in order that all elements of the Urban Design Plan can be implemented.

BATTLEFIELD CREEK BRIDGE PROJECT

- Architectural wall and railing system incorporating special lighting elements.
- Improve sidewalk, brick paving, bollards and new curbing.
- Tree trimming and removal of brush from Creek.

PEDESTRIAN MALL PROJECT

- Closure of Jones St. to through traffic at Bridge to Town Square.
- Installation of benches and planters on Bridge surface.
- Development of high quality pedestrian mall with amenities (brick pavement, landscaping, furnishings, archway enclosure, public art).
- Utility relocations (underground)
- Relocation of entrance to restaurant to public parking lot and establish connection from Mill Lane.
- Special landscaping features on south side of mall
- Park.

MEDIAN - BOULEVARD PROJECT

- Establish a landscaped median with lighting, between Second St. and First St.
- Architectural feature to denote entry into downtown core
- Extension of "downtown" streetlight, landscaping and sidewalks to Second St.
- Improvement of sidewalk, curbing to delineate on street parking, sidewalk boulevard landscaping from Second St. to Battlefield

PEDESTRIAN CROSSWALK PROJECT (PHASE 2)

- Village Green/Walker Ave.
- Second St.
- Introduce bike lanes if feasible.

PUBLIC PARKING PROJECT AT 27 JONES ST.

- Acquisition and site clean up.
- Parking lot paving.
- Entrance directly from King St.
- Parkette at rear and walkway along the Creek.

JONES ST. PEDESTRIANIZATION PROJECT

- Conversion of street with "woonerf" principles
- Visually enhancing pavement.
- Utility relocations (underground).
- Establish a landscaped median

KING STREET CANOPY PROJECT (SOUTH SIDE ONLY)

- Canopy features to visually connect facades of commercial buildings.

TOWN SQUARE PROJECT

- Property Acquisition. (complete).
- Extension of "downtown" streetlight, landscaping and sidewalks to New Mountain.
- Utility relocations (underground)
- Jones Rd. realignment.
- Site development (brick pavement, landscaping, furnishings, archway enclosure, architectural feature/clock tower/fountain).
- Relocation of a private driveway to Rosedale Dr.
- Crosswalk across King St. at New Mountain.

KING ST. BRIDGE PROJECT

- Widening sidewalk, brick paving, bollards and new curbing.
- Seat-wall, decorative railing system and new light standards.
- Tree trimming and removal of brush from Creek.
- Pedestrianize Crosswalk at Elm Ave.
- Eliminate driveway to apartment building from King St.
- Elimination of T.D. Bank driveway on King St.

JONES ST. BRIDGE PROJECT

- Pedestrianization of surface with brick paving.
- Decorative rail system and light standards.
- Acquisition of creek bed.
- Reconstruction of east bank of creek between King St. and Jones St.
- Construction of weir structure to retain water flow.

KING ST. SIDEWALK PROJECT (LAKE TO ELM)

- Sidewalk repairs and delineating on street parking with curbing.
- Additional Pedestrian features and street furniture/receptacles.
- Improve landscaping additional tree planting.

PEDESTRIAN CROSSWALK PROJECT (PHASE 1)

- Lake Ave.
- Mountain Ave.

SMITH'S KNOLL PROJECT

- Removal of portion of stone wall and restoration of remainder.
- Cannon relocation on appropriate bases.
- Utility relocation and new light standards.
- Entrance construction with stairway.
- Archeological Assessment.
- Site development, landscaping and furnishings (memorial restorations, benches, plaques, flag poles)
- Sidewalk reconstruction with brick pavers.
- Pedestrianize crosswalk at Battlefield Dr.
- Reconstruction of sidewalk from Battlefield Dr. Park as per downtown standard.
- Construction of connecting path through Park and provision of park benches.

THEME SIGNAGE PROJECT

- New street signs. (complete)
- New Parking and directional signs.
- Consolidation of Battlefield Park signage.
- Modify Sign By-law
- Establish fund for commercial facade/signage loan improvements program according to theme and regulations.

TOWN SQUARE BUILDING PROJECT

- Property Acquisition
- Site preparation.
- Development of New Building (10,000 sq. ft.).
- Potential uses – branch library, cultural centre/gallery, public storefront, leasable commercial etc.

BATTLEFIELD PARK PROJECT (PER MASTER PLAN)

- New entrance
- Enhancement of frontage streetscape
- Removal of all redundant structures
- Finalization landscape development.
- Interpretative Centre

SENIORS CENTRE PROJECT AT COMMUNITY PARK.

- Site preparation
- Development of New Building
- Leasable restaurant space
- Outdoor ice pad (winter)
- Public fountain (summer)

INNER SQUARE PROJECT IN JONES PEDESTRIAN MALL.

- Site development with public art and pedestrian features.
- Establish a landscaped median with lighting.
- Establish connection to King St.

PUBLIC PROJECTS IN CONJUNCTION WITH OLDE TOWN CENTRE PROJECT:

- Maintaining surface public parking lot
- Install landscaping, pedestrian links and street furniture in parking area.
- Acquire property for laneway.
- Disposal of surplus lands (i.e. Post Office, Fire Hall)
- Develop laneway and market stalls.
- Create market entrance feature from King St.
- Reconstruction of Mountain Ave. with on-street parking & loading.
- Arcaded/landscaped sidewalk on west side
- Establish sidewalk on east side.
- Interim relocation of branch library/senior's centre to OTC.
- Deposal of library property
- Interim relocation of Branch Library

PROJECTS RELYING ON PRIVATE/PUBLIC VENTURES

- General commercial facade and signage improvements according to theme and regulations.
- Various new buildings and enlargements in accordance with design principles suggested in elevation drawings and concept plans.
- Any redevelopment parallel to west bank of Stoney Creek should be high quality, with pedestrian link along creek.
- Any redevelopment on north side of Jones Street should have parking in rear.